

**PAROWAN CITY COUNCIL MEETING
DECEMBER 11, 2014
LIBRARY LOUNGE, 16 SOUTH MAIN, 6:00 P.M.**

MEMBERS PRESENT: Mayor Donald G. Landes, Councilmen Alan Adams, Troy Houston, Ben Johnson, Steve Thayer, Steve Weston, City Attorney Justin Wayment, City Manager Shayne Scott, City Recorder Callie Bassett

MEMBERS ABSENT: None.

PUBLIC PRESENT: Chuck and Dottie Stade, Richard and Marian Biasi, Shemree Naegele (Precise Tax) Greg and Vickie Hicks, Karen Hart, Also Biasi, Alesia Biasi, Ashlynn, Biasi, Averi Biasi, Ambree Biasi, Angelo Biasi, Mario Biasi, Mason Biasi, Dennis Gaede, Ken Allen, Von Mellor (Parowan Power Superintendent), Mike Strong, Donna Putich, Tom Scholes, Jeff Wood, Mike Adams, Judy Schiers (Parowan City Treasurer), Cleve Matheson (Parowan City Zoning Officer)

CALL TO ORDER: Mayor Landes called the meeting to order at 6:00 P.M.

OPENING CEREMONIES/THOUGHT/PRAYER – MAYOR LANDES: Mayor Landes opened the meeting with an invocation. He then led the Council and the public in the Pledge of Allegiance.

DOES ANYBODY HAVE ANY CONFLICTS OR PERSONAL INTEREST IN ANY MATTER ON THE AGENDA WHICH NEEDS TO BE DECLARED? No conflicts were declared.

CONSENT MEETING

APPROVAL OF MINUTES (OCT. 29, 2014 Special Council Meeting, NOV. 13, 2014 City Council Meeting and NOV. 20, 2014 Special City Council Meeting)

PURCHASE ORDERS/ WARRANT REGISTER

HANGAR LEASE AMENDMENT – Dan Doitch

SHADE TREE APPOINTMENTS – Vittorio Locatelli, Allissa Imming (alternate)

EMPLOYEE OF THE YEAR – Richard Biasi

Mr. Richard Biasi was voted Employee of the Year by his fellow employees. He was presented with a gift certificate from the Council. He is very deserving of this recognition. Mr. Biasi expressed his appreciation to his wife and children. He thanked Mr. Kelly Stones for hiring him and supporting through the years. He expressed how much he enjoys working with the other Parowan City employees.

Councilman Houston moved to approve the consent agenda. Councilman Weston seconded the motion. Mr. Scott explained the changes on the Hangar Lease Agreement. All Council members voted in favor of the motion. The consent agenda was approved.

PO# 878	Coast to Coast Carports	\$4,100.00
PO# 879	Scholzen	\$3,890.82
PO# 880	Music Theater International	\$1,800.00
PO# 881	Biasi Automotive	\$3,612.32

ACTION MEETING

AUDIT PRESENTATION/ACCEPTANCE – PRECISE TAX CPA'S:

Shemree Naegele thanked the staff and said they have done a very good job this year. She said the improvements over the last years have been drastic. She said in the past the City has had over 30 adjustments that need to be made during the audit. This year there were only a handful of adjustments.

Mrs. Naegele explained the financial statements to the Council. She started with the Audit Opinion Letter. It is a standard issue letter that all auditors give. Auditors give a clean opinion, a modified opinion, or they don't give an opinion. Parowan City received a clean opinion, which is very good.

Page 5 of the Financial Report is the beginning of the Management's Discussion and Analysis. Mrs. Naegele said that this is a good place to see comparison's from prior years and a good overview. She pointed out some financial highlights for the year:

- The City's net position as a whole increased by \$589,803. This means there were more inflows than outflows which increased the net position.
- The City's total unrestricted net position decreased by \$18,180.00. This means that \$18,000.00 more money was restricted for debt service or capital projects. Most of this was in the proprietary funds, or utility funds. Page 7 shows Parowan City's net Position and it breaks down the restricted and unrestricted funds.
- The total net position for governmental activities increased by \$100,724.00
- The total net position for business-type activities increased by \$489,079.00

Page 15 of the Financial Report is the Statement of Net Position and shows the government wide statements. Overall the City is healthy and the numbers look good. There are some areas that need some close attention.

Page 18 and 19 are the Governmental Funds which includes the Balance Sheet and Statement of Revenues & Changes in Fund Balance. This is basically the income statement which shows the revenues and expenses. This year for the general fund there was a change in fund balance (net income) of \$176,349.00. This increase left the City with an ending fund balance of \$403,609.00. Of this amount, \$334,561 is unrestricted. There is some that is restricted for capital projects and inventory. However, the park restroom project will need to come from the unrestricted amount. Mr. Scott added that the restrooms were budgeted for last year, but were not built. That budget amount carried over to this year. The budget will have to be opened to address this.

Mr. Scott asked if the \$403,609.00 is all cash. Mrs. Naegele said that \$334,561.00 is cash, and then there is inventory of \$32,927.00 and restricted funds of \$36,121.00. Mr. Scott also added that last year we received a finding because our general fund balance was too low. We have to have 5% - 25% of our general fund revenues in the fund balance. Mr. Scott said that some of the procedural changes that the Council has made have had a positive affect on the general fund.

Mrs. Naegele discussed the Capital Improvement fund, which includes bond proceeds of \$2.1 million dollars that came in right at the end of the year. Mr. Scott explained that we received a loan for the Main Street project to bridge the gap between the grant we received and what UDOT paid for. They gave us all of the proceeds of that loan and the money has been sitting on our books. They took the money from the joint highway commission and put it directly into the project. We have paid all the money we need to in the project, and now we need to turn this money back in. This will be done this fiscal year.

Page 22 is the Statement of Net Position of City's Proprietary Funds or Utility funds. It is an overview of the net position and shows the assets, the liabilities, and the restrictions in the net fund balance. In the Water Fund, there is quite a bit restricted for debt service and construction, and then there is an unrestricted balance in each of the funds. Page 23 shows the income loss before transfers. These numbers show how healthy these funds are. The service fees that came in and the expenses that came out is the net income. There were some transfers out to the general fund. Mrs. Naegele said there was a change this year from prior years. She said the City did a really good job of reallocating costs to accurately reflect where the costs were coming from and not just taking them from the general fund.

Mrs. Naegele pointed out the Electric fund had a healthy year. There was an ending net position of \$4.4 million dollars. She talked with Mr. Scott and he told her the Power Board has some ideas for projects to utilize these funds.

Mr. Scott thought the Council might find it interesting to look at the Water Fund. He pointed out that we "lost" \$33,000.00 in that fund this year because of the new well. He said we plan on losing much more because we are using fund balance to do the well. Page 22 shows a debt service of almost \$500,000.00. This is a sinking fund that we have to keep. We can't use that right away. The construction line shows that we used almost all of our impact fees for the well. There are unrestricted funds of \$228,000.00. We have used a lot of this water fund. We knew we were doing this. We did it on purpose. It is going along as we have designed it to do. Essentially what is happening is we are spending savings.

Mrs. Naegele pointed out on page 48 that in the non major governmental funds, the Cemetery Fund has a balance that is required to be held there. Besides this, there wasn't a whole lot of activity in these funds.

Page 51-52 is a detail of the City's non major proprietary funds – sewer treatment, garbage and pressurized irrigation funds. Mr. Houston asked why the Pressurized Irrigation fund is so healthy. Mrs. Naegele said that is mostly capital assets. The net position shows almost \$507,000 invested in capital assets, and \$26,600 in unrestricted funds. She pointed out that the Garbage Fund had a negative net position this year. This was due to the purchase of a garbage truck

during the fiscal year. The capital lease on the first garbage truck was paid off after the audit was done. This fund should become healthier. She doesn't think it will be a problem next year.

Page 55 is where the findings letters start. She said everything improved this year. Page 60 is where the actual findings start. Because there were still a few adjustments that were required (there were a couple of transactions that were material) she had to reissue the Misstatements Requiring Adjustments which is a material weakness that has been issued for a long time. She said it is close though. There were just two transactions that were material. She has gone over the transactions with management and staff and discussed different ways to approach these so they don't exist next year. This can be down graded to a significant deficiency next year or it may just go away.

The Significant Deficiency that Mrs. Naegele found is in the segregation of duties. She said that in a City our size, we are always going to have this finding. However, she did say that she did look at internal controls, and staff does a good job of mitigating the segregation of duties problems. There is oversight and cross training. So if anyone tried to commit fraud (not that anyone would) there would be oversight there and it would be caught. As the City grows and more people are hired, or if the boards step in and do more of the reviewing, this could go away, but it isn't one that is a big deal.

There was one compliance finding with the transparency website. Everything is up to date and current, but she was unable to reconcile the totals. She said she really believes it was cut off issues. Accountants like to see things in the year that they occurred. Sometimes things will be paid for in July, but on the financial statements they go back in to June. She thinks this was the problem, but she wasn't able to reconcile it, so she had to issue this finding. This isn't anything to be concerned about. She has discussed it with staff, and doesn't feel it will be an issue in the future.

Mr. Scott thought it would be a good thing for all Council members to have a copy of this Financial Report to refer to throughout the year. He will make copies for those who would like a hard copy.

Councilman Adams moved to accept the audit. Councilman Johnson seconded the motion with all Council members voting in favor of the motion. The motion was carried.

WATER CONSERVATION PLAN UPDATE – KELLY STONES: Mr. Stones had to leave the meeting. Mr. Scott said that this is a conservation plan that we are mandated to do every 5 years. This is an effort by state to conserve water. It provides interesting facts on how much water has been used. Mr. Stones had someone help put this together, so we didn't have to pay a consultant to do it.

Mayor Landes thinks this is a critical area for the community. Councilman Thayer commented that this describes our water sources as one chlorinated well and two springs. He said we have a new well now that obviously wouldn't have been put on this yet. He asked if the fore bay well is

City owned or farmer owned. He didn't know if this should be included in this report. He feels the City should know who owns that well.

Councilman Johnson asked where they got their information on future population growth. Mr. Scott said he thinks it was from the State.

Councilman Houston moved to table the water conservation plan. Councilman Johnson seconded the motion, with all Council members voting in favor of the motion. This item is tabled until a later date.

RECYCLE ZONE CREATION – ECONOMIC DEVELOPMENT COMMITTEE

RECOMMENDATION: Mr. Scott said that if it is ok to create this, they will create it. They received a recommendation from Economic Development committee to do so. It is very easy to do. This recycle “zone” will give business owners in the area an avenue for tax credits and other credits that are available out there. This area must be designated geographically, the same as an enterprise zone. Mr. Scott said he was going to use the same geography as the commercial zone.

Councilman Johnson moved to create a recycle zone for economic development as recommended by the Economic Development Committee. Councilman Thayer seconded the motion with all Council members voting in favor of the motion. The motion was carried.

401K CLARIFICATION FOR PAROWAN CITY POLICE OFFICERS: This was discussed at the last work meeting and was moved to the action meeting. A couple of different options were debated, but Mr. Scott said he wasn't sure there was a consensus last time.

Councilman Thayer said he has thought about this quite a bit. He said that because police officers get a much higher percent of contribution to their retirement, he thinks the City tried to make it fair to other City employees by kicking in an extra 5% match so they could be a little bit closer to where the police officers are. He thinks it was an effort back then to make it more fair, and that's why he thinks it is the way it is. He said if our former City Recorder, Valorie Topham, was still here she could give us a better history of what happened. But in looking at this, he feels this is what happened some years ago.

Councilman Weston reiterated Chief Carpenter's point that it is like comparing apples and oranges in terms of length of service, danger, 20 vs. 30 years, etc. Councilman Thayer said that the officers can continue to work after 20 years and add to their retirement. Councilman Weston agreed, but said that this is how the system was set up.

Councilman Weston said it has been so long that he has forgotten the figures. Councilman Johnson said that it basically boiled down to making a percentage change, simply matching what other employees are receiving, or leaving it the same.

Councilman Thayer moved to leave it the same as it has always been done. There was no second on the motion. The motion failed.

Councilman Houston moved to make the matching for police officers the same as all other City employees. Councilman Weston seconded the motion. Councilmen Adams, Houston, Johnson, and Weston voted in favor of the motion. Councilman Thayer voted against the motion. The majority vote carried the motion.

A RESOLUTION REPEALING THE CITY'S OCTOBER 23, 2014 BOND RESOLUTION AND AUTHORIZING \$3,464,000 ELECTRIC REVENUE BONDS, SERIES 2014 (THE "SERIES 2014 BONDS"); AUTHORIZING A GENERAL INDENTURE OF TRUST, A SECOND SUPPLEMENTAL INDENTURE OF TRUST, AND OTHER DOCUMENTS; AND RELATED MATTERS – RESOLUTION NO. 2014-12-01: We have done this before, but the last time they had the wrong trustee on the documents. All this is doing is changing the trustee from Wells Fargo to Zion's Bank.

Councilman Weston moved to approve resolution 2014-12-01. Councilman Johnson seconded the motion. A roll call vote was taken as follows:

	<u>Aye</u>	<u>Nay</u>	<u>Absent</u>	<u>Abstain</u>
Councilman Alan Adams	X			
Councilman Troy Houston	X			
Councilman Ben Johnson	X			
Councilman Steve Thayer	X			
Councilman Steve Weston	X			

Resolution 2014-12-01 was passed.

WORK MEETING

IMPACT FEE CREDITS APPLICATION – MR. KEN ALLEN: Mr. Cleve Matheson explained that Mr. Allen did a lot split in the Sunset View subdivision. He put in a lot of improvements. Mr. Matheson said he completed what should have been completed when the subdivision was created. These improvements included finishing the road, curb and gutter, sidewalks, asphalt, a completed sewer line, a new man hole, upgrading a 2" water line to an 8" water line with a hydrant at the end, plus a temporary cul-de-sac for a turn around for emergency vehicles and our garbage truck. Mr. Allen talked with Mr. Matheson and Mr. Matheson said he supports Mr. Allen in asking the Council that he receive impact fee credits for his new home in the amount of \$6,838.05, not to exceed 5 years.

Mr. Scott asked how much of what Mr. Matheson just explained was required. Mr. Matheson said that none of these improvements had to be done right now. Mr. Allen said the reason he did the water line now was because they wanted to pave the street and put curb and gutter in, and he didn't want to have to come back and cut the road at a later date.

Councilman Houston said he would like to know where the development stopped and started. He said the street comes around and ends at the chain link fence. He asked if the subdivision ended at that house. Mr. Allen said it did. Mr. Houston then said Mr. Allen basically needed to improve that road so he could get a building permit. Mr. Matheson said that the improvements

in that subdivision never made it to the boundaries. They all stopped about 60-70 feet short of the boundaries. Mr. Matheson said that now they are connected.

Mr. Scott said that one of the arguments Mr. Matheson and he talked about is that the purpose of impact fees is to not impact our existing residents with new development. They feel that Mr. Allen has gone above and beyond by putting in things that are not only going to benefit him but other developments as well.

Mr. Allen said he spent a lot of money bringing in all of these improvements. He is simply asking that the Council give him a break on the impact fees. Councilman Johnson asked if this has been through Planning and Zoning. Mr. Scott said that this request has not, and should not. Mayor Landes said he feels Mr. Allen has already lived up to the purpose of the impact fees. Mr. Scott said he thinks this is very clear to staff and would agree.

Councilman Houston thinks that to be fair there should be a percentage – i.e., if 40 % was in the old area and 60% was in the new development area, then the 60% costs should be the responsibility of the land owner to provide that service if it wasn't there. The 40% could be because he upgraded it for the City. Mr. Wayment asked Mr. Allen if he knows what percentage of the work he did was in the old section. Councilman Houston said this is what he wants to see. Mr. Wayment said we should move this on to action, and show the numbers from old part vs. new part. Allocated costs in each section to justify the impact fee credit. Mr. Matheson said he will get the numbers for the next meeting. Mr. Scott said he will also write a letter dictating to Mr. Allen what he is getting and what the terms are. This agenda item will be moved to the next action meeting.

CENTER CREEK HYDRO UPDATE – SUNRISE ENGINEERING: Mr. Jesse Ralphs updated the Council on the progress of the Center Creek Project with a power point presentation.

ROAD AGREEMENT WITH ULCT/SALT LAKE CHAMBER OF COMMERCE: This is basically a coalition of organizations working together to try to raise funds for street improvement throughout the state. This would be a meaningful thing for us because we need additional funds. We receive \$140,000 a year for our streets, and we spend well over \$200,000.

Councilman Adams said he understands that we would need to contribute \$100 to join this coalition. He asked if there are any other expenses. He couldn't see that there are. Mr. Scott said that they want us to take action in the form of a resolution that says we support the effort to raise more funding for transportation. They are going to put the hard sell on the legislature this year.

Councilman Thayer asked if this is to have a gas tax increase. Mayor Landes said yes. Mr. Thayer said that depending on what the increase it could double or triple our revenues so we would have more money to work on our own roads. Mayor Landes said this would be very beneficial for our own roads.

Attorney Wayment wondered if this is really going to benefit us down here. He said usually these things benefit the Wasatch front where most of the population is. Mr. Scott said it will benefit us.

This agenda item will be moved to the next action meeting.

UAMPS RESOURCE COMMITTEE DISCUSSION: This was discussed at the last meeting. The argument for joining this resource committee is that there is a war on coal and we are 60% in coal. Someday we will have to do something different. The argument against is that it will cost \$700/month and we may be doing something entirely different than what that resource committee finds to do. Maybe we can even get in at a different time.

Mr. Scott said Mr. Mellor and he have gone back and forth. Mr. Scott thinks we should wait. Mr. Mellor said the only reason we should get involved is if we want to get involved with nuclear. Mr. Mellor's recommendation is that we should wait, unless we want to get into nuclear.

Councilman Thayer said that solar power could be an option in the future. Mr. Mellor thinks there is going to be more and more pressure to get into renewables. The consensus of the City Council is that we wait.

MEMBER REPORTS:

Councilman Houston stated that other people have built homes and have been required to put in improvements like fire hydrants. He wants to be fair to everyone in the City. Attorney Wayment said this is why he suggests a line extension agreement.

Councilman Adams didn't have anything to report this evening.
Councilman Johnson didn't have anything to report this evening.

Councilman Weston reported that the Shade Tree Committee is taking out undesirable trees around town. The Community Theater is doing "1776" this March. The Historical Committee has been meeting to put some input on the New City Hall to make sure it fits the historic character of our town.

Councilman Thayer reported that the Patchwork Parkway held a grant writing seminar that was quite successful.

PUBLIC COMMENT & DISCUSSION – TWO MINUTES EACH:

Mr. Mike strong said that 3 or 4 people from Parowan attended the grant writing session. He said there was a lot of good information given.

Mr. Chuck Stade asked who chip seals our streets. Mr. Scott said we hire a contractor. He asks who is in charge and why do they do the best streets in town? He said that Mr. Scott said there is a science behind it. Mr. Stade is surprised that people living at the end of the streets aren't complaining. Councilman Johnson said that sometimes you have to save the roads that you can

save. Councilman Houston said that we are currently paying for a study to be done on the roads so we will have a plan.

Mike Strong said that at the grant writing seminar they said that Parowan got a grant for \$35,000. He asked where we are on that. Mr. Scott said we haven't received the contract from UDOT yet. This has been put out to bid and will be reviewed by TARP.

Councilman Houston moved to go into closed session at 7:40 p.m. Councilman Thayer seconded the motion, with all Council members voting in favor of the motion.

**CLOSED SESSION – STRATEGY SESSION TO DISCUSS THE PURCHASE,
EXCHANGE OR LEASE OF REAL PROPERTY:**

Those in attendance at the closed session were Mayor Landes, Councilmen Alan Adams, Troy Houston, Ben Johnson, Steve Thayer and Steve Weston, City Attorney Justin Wayment, City Manager Shayne Scott, and City Recorder Callie Bassett.

The Council moved out of closed session at 8:00 p.m.

ADJOURN: Councilman Houston moved to adjourn the meeting. Councilman Adams seconded the motion, with all Council members voting in favor of the motion. The meeting was adjourned at 8:02 p.m.

Parowan, UT 84761
www.parowan.org

+	-

Alan Adams, Councilman
Ben Johnson, Councilman
Troy Houston, Councilman
Steve Thayer, Councilman
Steve Weston, Councilman

DATE January 8, 2015

[illegible]

**NOTE: If there is a negative vote please note specifically the individual item that was opposed.

PAROWAN CITY CORPORATION

5 SOUTH MAIN • P.O. BOX 576
PAROWAN, UT 84761-0576
(435) 477-3331

THIS ORDER NUMBER MUST APPEAR
ON ALL PACKAGES, INVOICES AND
SHIPPING PAPERS

TO TWIN "D", INC.

PURCHASE ORDER
NO. 0883

DATE: 12/23/2014

DELIVERY ADDRESS:

DEPARTMENT			ACCT. NO.		
SEWER			524031		
ITEM NO.	QUANTITY	UNIT	DESCRIPTION	ESTIMATED UNIT PRICE	ESTIMATED AMOUNT
			VIDEO INSPECTION, FLUSHING & VACUUMING		\$20,045.00
			OF SEWER MAINS / PER DIEM FOR		
			CREW.		

Kelly Stones
DEPARTMENT HEAD
Calthe Bassett
CITY RECORDER

Jimmy Schiers
CITY TREASURER
Shayla Scott
CITY MANAGER

☐ APPROVED BY CITY COUNCIL 1-8-15

TWIN "D" INC.

ENVIRONMENTAL SERVICES

3038 North 750 East • Layton, Utah 84041
Phone (801) 771-3038 • Fax (801) 771-3040
Toll Free: 866-337-9263

Invoice

DATE	INVOICE NO.
11/27/2014	12881

BILL TO
PAROWAN CITY P.O. BOX 576 PAROWAN, UT 84761

Twin "D" Operator		P.O. # / APPROVED BY	TERMS	DUE DATE	PROJECT	
BOO / JEFF		CONTRACT	Net 30	12/27/2014	2014-15 SEWER MAINTENANCE	
ITEM	DESCRIPTION			QTY	RATE	AMOUNT
MC	TRAVEL CHARGE FOR CLEANING OR VAC TRUCK			1	400.00	400.00
MV	TRAVEL CHARGE FOR TV TRUCK			1	350.00	350.00
MCF	MUNICIPAL FLUSHING AND VACUUMING OF EXISTING 6" & 8" SANITARY SEWER MAINS - PER FOOT RATE			17,169.4	0.43	7,382.84
MCF	MUNICIPAL FLUSHING AND VACUUMING OF EXISTING 10" SANITARY SEWER MAINS - PER FOOT RATE			1,621.6	0.46	745.94
MCH	MUNICIPAL FLUSHING AND VACUUMING OF AN EXTREME CONDITION SECTION TO REMOVE AN RV HOSE - HOURLY RATE			1.25	200.00	250.00
MVF	MUNICIPAL VIDEO INSPECTION OF EXISTING 6" SANITARY SEWER MAINS - PER FOOT RATE			5,668	0.61	3,457.48
MVF	MUNICIPAL VIDEO INSPECTION OF EXISTING 8" & 10" SANITARY SEWER MAINS - PER FOOT RATE			12,664.2	0.51	6,458.74
PER DIEM	DAILY CHARGE FOR CREW OVERNIGHT EXPENSES			8	125.00	1,000.00
<div>Date Invoiced Received <u>12/23/2014</u> Department <u>Sewer Collection</u> GL Code to be Paid <u>52 40 31</u> Approval by Department Head <u>[Signature]</u></div>						
				<div>Total\$20,045.00</div>		

COPY

Taking Care of the World's Environment . . . One Pipe at a Time

PAROWAN CITY CORPORATION

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PAROWAN, UT 84761-0576
(435) 477-3331

THIS ORDER NUMBER MUST APPEAR
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TO RICHARD BIASI

PURCHASE ORDER

NO. 0884

DATE: 12/30/2014

DELIVERY ADDRESS:

DEPARTMENT

SEWER TREATMENT

ACCT. NO.

544030

ITEM NO.	QUANTITY	UNIT	DESCRIPTION	ESTIMATED UNIT PRICE	ESTIMATED AMOUNT
			LAND DISPOSAL OF SEWER EFFLUENT		# 1975.1
			FOR 2014 IRRIGATION SEASON		

DEPARTMENT HEAD

Chloe Barnett

CITY RECORDER

CITY TREASURER

Greg Scott

CITY MANAGER

☐ APPROVED BY CITY COUNCIL 1-8-15

Richard Biasi

INVOICE

336 North 300 West
Parowan Ut, 84761
Phone 590-1161

INVOICE # ~~8~~ ⁴
DATE: DECEMBER 30, 2014

TO:
Parowan City Corp
05 South Main
Parowan Ut, 84761
477-3331

FOR:
Land Disposal of Sewer effluent 2014 Irrigation Season

DESCRIPTION	Days	RATE	AMOUNT
Land disposal of Sewer Effluent for 2014 Irrigation Season	145	55.00	7975.00
TOTAL			\$7975.00

Make all checks payable to Richard Biasi
Total due by Jan25, 2015 Overdue accounts subject to a service charge of 1% per month.

COPY

Thank you for your business!

544030

Parowan City Lagoons

Land Disposal

Disposal Begin Date May 1 2014

Disposal End Date Sept 30, 2014

Total Number of Days 145

I, Aldo Biasi as lagoon operator certify that
the above information is correct with regards to how many days
Parowan City disposed of sewer effluent on Biasi Farms for the
2014 irrigation season

Signature Aldo Biasi Date 12-30-14

COPY

PAROWAN CITY CORPORATION

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PAROWAN, UT 84761-0576
(435) 477-3331

THIS ORDER NUMBER MUST APPEAR
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TO WHEELER MACHINERY

PURCHASE ORDER
NO. 0885

DATE: 12/31/2014

DELIVERY ADDRESS:

DEPARTMENT			ACCT. NO.		
CLASS "C" ROADS			106157		
ITEM NO.	QUANTITY	UNIT	DESCRIPTION	ESTIMATED UNIT PRICE	ESTIMATED AMOUNT
			WHEEL LOADER YEARLY RENTAL (SNOW REMOVAL)		\$1,605.5

Kelly Stones
DEPARTMENT HEAD
Calvin Barnett
CITY RECORDER

James Smith
CITY TREASURER
Shay Scott
CITY MANAGER

☐ APPROVED BY CITY COUNCIL 1-8-15

WHEELER

CAT Rental
STORERENTAL / SALES
INVOICE451 North Lund Hwy
Cedar City, UT (435) 586-6323Please Remit To:
PO Box 413071
Salt Lake City, Utah 84141-3071

SOLD TO

SHIP TO

PAROWAN CITY CORPORATION
PO BOX 576
PAROWAN UT

84761

IRON COUNTY UTAH
PAROWAN
PAROWAN CITY YARD
SNOW REMOVAL

INVOICE NUMBER RS0000021377	INVOICE DATE 12-30-14	CUSTOMER NUMBER 013594	CUSTOMER ORDER NUMBER KELLY STONE	STORE 10	DIV G	SALESMAN 242	TERMS 2	PAGE 1
AGREEMENT NO. 106850	DOC. DATE 11-24-14	PC	LC	MC 10	SHIP VIA TXX PPI			INV SEQ NO. 338928
MAKE AA	MODEL 924K QCF	SERIAL NUMBER PWR02728			EQUIPMENT NUMBER	METER READING 268.0	MATCHID NO. LKE02340	
QUANTITY	ITEM	*N/R	DESCRIPTION			UNIT PRICE	EXTENSION	

CUSTOMER CONTACT: KELLY STONE

TAX EXEMPTION LICENSE 11710968-002STC

EQUIPMENT RENTAL

FROM 11/24/14 THRU 12/21/14

CATERPILLAR

MODEL 924K QCF

WHEEL LOADER 924

1.0 ID NO: LKE02340

SERIAL NO: PWR02728

1296.00

PIN: *CAT0924KCPWR02728*

FOB: CEDAR

1.0 HAULAGE EXTERNAL

300.00

1.0 ENVIRO FEE

9.50

Date Invoiced Received 12/31/2014

Department _____

GL Code to be Fld _____

Approval by Department Head _____

COPY

* Non Returnable

A monthly finance charge of 2% is assessed on all past due invoices on the last day on each month. In the event this invoice remains unpaid, the customer is responsible for all attorney/collection fees and costs. For any questions please contact 801-974-0511

PAY THIS

AMOUNT ►

1,605.50

AMOUNT

CREDITED ►

DUE DATE: 01-29-2015

Visit us online... www.WheelerCat.com

PAROWAN CITY CORPORATION

5 SOUTH MAIN • P.O. BOX 576
PAROWAN, UT 84761-0576
(435) 477-3331

THIS ORDER NUMBER MUST APPEAR
ON ALL PACKAGES, INVOICES AND
SHIPPING PAPERS

TO BEST DEAL SPRINGS

PURCHASE ORDER

NO. 0886

DATE: 12/31/2014

DELIVERY ADDRESS:

DEPARTMENT

CLASS "C" ROAD

ACCT. NO.

106125

ITEM NO.	QUANTITY	UNIT	DESCRIPTION	ESTIMATED UNIT PRICE	ESTIMATED AMOUNT
			POWER DIVIDER & CORE		\$ 2,102.5
			'80 AUTOCAR (DUMP TRUCK)		

DEPARTMENT HEAD

CITY RECORDER

CITY TREASURER

CITY MANAGER

☐ APPROVED BY CITY COUNCIL 1-8-15



& Truck Parts

SINCE 1950

www.bestdealspring.com

Best Deal Spring, Inc.

444 East 100 North Payson, UT 84651
 155 East 4400 South Price, UT 84501
 1437 South 270 East, Ste. 2 St. George, UT 84790
 2364 East Highway 40 Vernal, UT 84078
 Ph: 801-465-4873 Ph: 435-637-4501 Ph: 435-652-8933 Ph: 435-789-7044
 Fx: 801-465-0866 Fx: 435-637-0402 Fx: 435-652-8970 Fx: 435-789-7046

Please Remit All Payments to: 444 East 100 North, Payson, UT 84651



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PAR10
PAROWAN CITY
PO BOX 576
PAROWAN, UT 84761 USA

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PAR10-1
PAROWAN CITY
400 N. MAIN
PAROWAN, UT 84761 USA

Phone:

Order Ref. No. W3*355990	Ctrl.No. RICK	Time 11:28am	Veh. No.	CHG INVOICE REPRINT	Page 1	Invoice No. 327439
Customer P.O. No.	SNO RICK	Payment Terms NET 30 DAYS	Invoice Date 12-31-14	Shipped Via RICK DELIVER		

Line	Ord	Ship	B/O	Part Number	Description	Unit Price	Extended Price
1	1	1	0	BOT 38D	POWERDIVIDER	1662.50	1,662.50
					Core	300.00	300.00

Parts: 1,662.50

Cores: 300.00

Freight In: 140.00

Invoice Total: \$2,102.50

COPY

OFFICE COPY

\$2,102.50



PARTS SOLD HEREIN ARE WARRANTED ONLY TO THE EXTENT OF THE WARRANTY FURNISHED BY THE MANUFACTURER OF SUCH PARTS. ITEMS RETURNED MUST BE IN NEW CONDITION, ORIGINAL BOX AND ACCOMPANIED BY THE PURCHASE INVOICE. THERE WILL BE NO REFUNDS OR REFUNDS ALLOWED ON THE FOLLOWING: 1) PARTS RETURNED AFTER 30 DAYS; 2) ANY ELECTRICAL EQUIPMENT OR PARTS; 3) BOLTS; 4) BROKEN OR PARTIAL GTS AND CARTRIDGE SETS; 5) TOOLS THAT HAVE BEEN USED; 6) SPECIAL ORDER ITEMS; OR 7) PARTS NOT IN THEIR ORIGINAL PACKAGE. 2% CHARGE ON INVOICES 30 DAYS PAST DUE (24% ANNUALLY). PAST DUE ACCOUNTS MAY BE PLACED ON C.O.D. WITHOUT NOTIFICATION. CORE RETURNS MUST BE IDENTICAL TO UNIT PURCHASED, ASSEMBLED AND OIL CHANGED OF ALL LUBRICANTS. CORES MUST BE RETURNED IN THE ORIGINAL BOX WITHIN 30 DAYS FROM THE DATE OF PURCHASE.

THANK YOU! WE APPRECIATE YOUR BUSINESS.

PAROWAN CITY CORPORATION

5 SOUTH MAIN • P.O. BOX 576
PAROWAN, UT 84761-0576
(435) 477-3331

THIS ORDER NUMBER MUST APPEAR
ON ALL PACKAGES, INVOICES AND
SHIPPING PAPERS

TO IRON GATE BUILDERS

PURCHASE ORDER
NO. 0887

DATE: 1/5/2015

DELIVERY ADDRESS:

DEPARTMENT

FIRE DEPT.

ACCT. NO.

105749

ITEM NO.	QUANTITY	UNIT	DESCRIPTION	ESTIMATED UNIT PRICE	ESTIMATED AMOUNT
			NEW GARAGE DOORS AT FIRE HOUSE		\$6,224.11

DEPARTMENT HEAD

CITY RECORDER

CITY TREASURER

CITY MANAGER

☐ APPROVED BY CITY COUNCIL 1-8-15



fine homebuilding

NATHAN THAYER, PRESIDENT

435.704.1474

P.O. Box 1660

PAROWAN UTAH, 84761

Date: 12-10-2014
Customer: Parowan city Home #: _____
Street: _____ Cell #: _____
City/State/Zip _____ Work #: _____

Contractor hereby submits the following specifications:
Install (20 steel backed 12'x12' Martin garage doors at fire station.
Includes openers and weather strip

As stated above in the specifications, we propose to furnish material and labor for the sum of: (\$6,224.18).

Any alteration or deviation from the above specifications involving extra costs will be executed only upon a written order and will become an extra charge over and above the proposed estimate.

Acceptance of Proposal

As stated in the above specifications, the costs, materials and specifications are satisfactory and are hereby accepted. I authorized the contractor to perform the work as specified and payments will be made as summarize above.

Signature: _____ Date: _____

COPY

IRON COUNTY
HBA
HOME BUILDERS ASSOCIATION

PAROWAN CITY CORPORATION

5 SOUTH MAIN • P.O. BOX 576
PAROWAN, UT 84761-0576
(435) 477-3331

THIS ORDER NUMBER MUST APPEAR
ON ALL PACKAGES, INVOICES AND
SHIPPING PAPERS

TO PRECISE TAX & ACCOUNTING, LLC

PURCHASE ORDER

NO. 0888

DATE: 1/5/2015

DELIVERY ADDRESS:

DEPARTMENT			ACCT. NO.		
<u>AUDIT</u>			<u>AUDIT SPLIT</u>		
ITEM NO.	QUANTITY	UNIT	DESCRIPTION	ESTIMATED UNIT PRICE	ESTIMATED AMOUNT
			SERVICES FOR 2013-2014 AUDIT		\$ 2,725.00
			125 @ \$95 = 11,875		
			PRIOR BILLING = 6,175.00		
			DISCOUNT - 2,975.00		
			<u>\$ 2,725.00</u>		

DEPARTMENT HEAD
Chloe Bassett
CITY RECORDER

Judy Schuers
CITY TREASURER
Shay So #
CITY MANAGER

☐ APPROVED BY CITY COUNCIL 1-8-15



Invoice

Invoice #: D1556

Invoice Date: 12/30/2014

Due Date: 1/29/2015

Bill To:

Parowan City
PO Box 576
Parowan, UT 84761

Contact Information

970 S Sage Drive, Ste. 102
Cedar City, UT 84720

Phone: 435-267-0144

Fax: 435-267-0128

E-mail: derrick@precisetaxcpas.com

Description	Hours/Qty	Rate	Amount
Financial Audit billing for Professional Auditing services performed for the 2013-2014 period. 125@ \$95= \$11,875.	125	95.00	11,875.00
Prior Billing		-6,175.00	-6,175.00
Professional Discount		-2,975.00	-2,975.00
<div>COPY</div>			

Please Make Checks Payable to Precise Tax & Accounting, LLC. Credit Cards accepted over the phone. 435-267-0144

Thank You For Your Business!

Total \$2,725.00

Payments/Credits \$0.00

Balance Due \$2,725.00

CM5

**Parowan City
Check Register
General Checking - 12/09/2014 to 01/06/2015**

Payee Name	Reference Number	Invoice Number	Payment Date	Amount	Description	Ledger Account
5 Star Life Insurance Company	24774	PR121214-3901	12/31/2014	156.07	5 Star Life Insurance	102245 - MISC/PAYROLL PAYAB
5 Star Life Insurance Company	24774	PR122614-3901	12/31/2014	156.07	5 Star Life Insurance	102245 - MISC/PAYROLL PAYAB
				\$312.14		
ADAMS, ALAN	24687	12082014	12/12/2014	280.00	TRAVEL EXPENSE REIMBURSEMENT FOR UA	534023 - TRAVEL, MEALS AND L
AFLAC	24775	PR121214-3750	12/31/2014	18.30	AFLAC after-tax	102253 - AMERICAN FAMILY LIF
AFLAC	24775	PR121214-3750	12/31/2014	61.08	AFLAC pre-tax	102253 - AMERICAN FAMILY LIF
AFLAC	24775	PR122614-3750	12/31/2014	18.30	AFLAC after-tax	102253 - AMERICAN FAMILY LIF
AFLAC	24775	PR122614-3750	12/31/2014	61.08	AFLAC pre-tax	102253 - AMERICAN FAMILY LIF
				\$158.76		
ALSCO-AMERICAN LINEN DIVISIO	24688	LSTG611750	12/12/2014	35.95	MAT CLEANING SERVICE	524026 - MAINTENANCE MATERI
ALSCO-AMERICAN LINEN DIVISIO	24688	LSTG611750	12/12/2014	35.95	MAT CLEANING SERVICE	524026 - MAINTENANCE MATERI
ALSCO-AMERICAN LINEN DIVISIO	24759	LSTG613888	12/23/2014	35.95	MAT CLEANING SERVICE	524026 - MAINTENANCE MATERI
ALSCO-AMERICAN LINEN DIVISIO	24759	LSTG613888	12/23/2014	35.95	MAT CLEANING SERVICE	524026 - MAINTENANCE MATERI
				\$143.80		
ARMSTRONG CONSULTANTS, INC	24689	14-146189-01	12/12/2014	10,500.00	PROJECT #1-RUNWAY 4/22, APRON AND TAXI	444031 - ENGINEERING
BEST DEAL SPRING, INC	24690	326806	12/12/2014	383.40	parts for '04 Condor - slack adjusters & brake can	554025 - REPAIR TO EQUIPMEN
BEST DEAL SPRING, INC	24729	326978	12/19/2014	258.24	'04 CONDOR SHOCK ABSORBERS	554025 - REPAIR TO EQUIPMEN
BEST DEAL SPRING, INC	24729	327067	12/19/2014	-312.52	CREDIT MEMO - RETURNED SLACK ADJUSTE	554025 - REPAIR TO EQUIPMEN
BEST DEAL SPRING, INC	24729	327081	12/19/2014	31.02	MUD FLAPS	106125 - REPAIR TO EQUIPMEN
BEST DEAL SPRING, INC	24729	327106	12/19/2014	85.46	FRONT WHEEL SEALS FOR '04 CONDOR	554025 - REPAIR TO EQUIPMEN
				\$445.60		
BIASI AUTOMOTIVE & DIESEL, INC	24691	30104	12/12/2014	3,612.32	repairs on 2007 Dodge - Pickup R2500	106125 - REPAIR TO EQUIPMEN
BIASI AUTOMOTIVE & DIESEL, INC	24691	30195	12/12/2014	188.71	BRAKE REPAIR ON 2001 FORD F 250 SUPER D	544025 - REPAIRS TO EQUIPME
BIASI AUTOMOTIVE & DIESEL, INC	24760	30186	12/23/2014	2,232.38	MAJOR BRAKE REPAIR FOR 2001 FORD F 350	514025 - REPAIR TO EQUIPMEN
				\$6,033.41		
BONNEVILLE INDUSTRIAL SUPPL	24692	2542620-1	12/12/2014	7.33	SHOP SPLIT	106126 - MAINTENANCE, MATER
BONNEVILLE INDUSTRIAL SUPPL	24692	2542620-1	12/12/2014	7.33	SHOP SPLIT	514026 - MAINTENANCE MATERI
BONNEVILLE INDUSTRIAL SUPPL	24692	2542620-1	12/12/2014	7.33	SHOP SPLIT	524026 - MAINTENANCE MATERI
BONNEVILLE INDUSTRIAL SUPPL	24692	2542620-1	12/12/2014	7.33	SHOP SPLIT	544026 - MAINTENANCE MATERI
BONNEVILLE INDUSTRIAL SUPPL	24692	2542620-1	12/12/2014	7.33	SHOP SPLIT	574026 - MAINTENANCE MATERI
BONNEVILLE INDUSTRIAL SUPPL	24692	2542620-1	12/12/2014	7.35	SHOP SPLIT	534026 - MAINTENANCE MATERI
BONNEVILLE INDUSTRIAL SUPPL	24730	2543056	12/19/2014	4.60	SHOP SPLIT	534026 - MAINTENANCE MATERI
BONNEVILLE INDUSTRIAL SUPPL	24730	2543056	12/19/2014	4.61	SHOP SPLIT	106126 - MAINTENANCE, MATER
BONNEVILLE INDUSTRIAL SUPPL	24730	2543056	12/19/2014	4.61	SHOP SPLIT	514026 - MAINTENANCE MATERI
BONNEVILLE INDUSTRIAL SUPPL	24730	2543056	12/19/2014	4.61	SHOP SPLIT	524026 - MAINTENANCE MATERI
BONNEVILLE INDUSTRIAL SUPPL	24730	2543056	12/19/2014	4.61	SHOP SPLIT	544026 - MAINTENANCE MATERI
BONNEVILLE INDUSTRIAL SUPPL	24730	2543056	12/19/2014	4.61	SHOP SPLIT	574026 - MAINTENANCE MATERI
BONNEVILLE INDUSTRIAL SUPPL	24730	2543370	12/19/2014	19.67	SHOP SPLIT	106126 - MAINTENANCE, MATER
BONNEVILLE INDUSTRIAL SUPPL	24730	2543370	12/19/2014	19.67	SHOP SPLIT	524026 - MAINTENANCE MATERI
BONNEVILLE INDUSTRIAL SUPPL	24730	2543370	12/19/2014	19.67	SHOP SPLIT	544026 - MAINTENANCE MATERI
BONNEVILLE INDUSTRIAL SUPPL	24730	2543370	12/19/2014	19.67	SHOP SPLIT	574026 - MAINTENANCE MATERI
BONNEVILLE INDUSTRIAL SUPPL	24730	2543370	12/19/2014	19.68	SHOP SPLIT	514026 - MAINTENANCE MATERI
BONNEVILLE INDUSTRIAL SUPPL	24730	2543370	12/19/2014	19.72	SHOP SPLIT	534026 - MAINTENANCE MATERI
				\$189.73		

Parowan City
Check Register
General Checking - 12/09/2014 to 01/06/2015

Payee Name	Reference Number	Invoice Number	Payment Date	Amount	Description	Ledger Account
BOWEN COLLINS & ASSOCIATES,	24731	12080	12/19/2014	144.50	300 East Well Equipping Project - Project # 284-1	511601 - CONSTRUCTION IN PR
BRINGHURST DAVE	24732	119713001.1212	12/19/2014	196.94	Deposit Refund: 119713001 - BRINGHURST DAV	532135 - CUSTOMER DEPOSITS
BUSINESS SOLUTIONS GROUP	24733	12969	12/19/2014	1.62	OFFICE SUPPLIES SPLIT	104224 - OFFICE SUPPLIES AND
BUSINESS SOLUTIONS GROUP	24733	12969	12/19/2014	1.62	OFFICE SUPPLIES SPLIT	105824 - OFFICE SUPPLIES AND
BUSINESS SOLUTIONS GROUP	24733	12969	12/19/2014	1.62	OFFICE SUPPLIES SPLIT	106926 - MAINTENANCE MATERI
BUSINESS SOLUTIONS GROUP	24733	12969	12/19/2014	1.62	OFFICE SUPPLIES SPLIT	108026 - MAINTENANCE MATERI
BUSINESS SOLUTIONS GROUP	24733	12969	12/19/2014	3.23	OFFICE SUPPLIES SPLIT	104124 - OFFICE SUPPLIES AND
BUSINESS SOLUTIONS GROUP	24733	12969	12/19/2014	3.23	OFFICE SUPPLIES SPLIT	105926 - MAINTENANCE MATERI
BUSINESS SOLUTIONS GROUP	24733	12969	12/19/2014	8.08	OFFICE SUPPLIES SPLIT	105424 - OFFICE SUPPLIES AND
BUSINESS SOLUTIONS GROUP	24733	12969	12/19/2014	11.28	OFFICE SUPPLIES SPLIT	104324 - OFFICE SUPPLIES AND
BUSINESS SOLUTIONS GROUP	24733	12969	12/19/2014	15.30	OFFICE SUPPLIES SPLIT	524024 - OFFICE SUPPLIES AND
BUSINESS SOLUTIONS GROUP	24733	12969	12/19/2014	15.30	OFFICE SUPPLIES SPLIT	544024 - OFFICE SUPPLIES AND
BUSINESS SOLUTIONS GROUP	24733	12969	12/19/2014	17.00	OFFICE SUPPLIES SPLIT	574026 - MAINTENANCE MATERI
BUSINESS SOLUTIONS GROUP	24733	12969	12/19/2014	25.50	OFFICE SUPPLIES SPLIT	554024 - OFFICE SUPPLIES AND
BUSINESS SOLUTIONS GROUP	24733	12969	12/19/2014	30.60	OFFICE SUPPLIES SPLIT	514024 - OFFICE SUPPLIES AND
BUSINESS SOLUTIONS GROUP	24733	12969	12/19/2014	34.00	OFFICE SUPPLIES SPLIT	534024 - OFFICE SUPPLIES AND
				\$170.00		
CAL RANCH STORES	24693	4496/11	12/12/2014	429.99	TRK BOX ALM SGL LID LOW	534025 - REPAIR TO EQUIPMEN
CENTURY LINK	24734	338312172014	12/19/2014	3.99	CENTURY LINK SPLIT	105728 - TELEPHONE
CENTURY LINK	24734	338312172014	12/19/2014	3.99	CENTURY LINK SPLIT	105828 - TELEPHONE
CENTURY LINK	24734	338312172014	12/19/2014	3.99	CENTURY LINK SPLIT	106928 - TELEPHONE
CENTURY LINK	24734	338312172014	12/19/2014	3.99	CENTURY LINK SPLIT	107128 - TELEPHONE
CENTURY LINK	24734	338312172014	12/19/2014	4.03	CENTURY LINK SPLIT	108028 - TELEPHONE
CENTURY LINK	24734	338312172014	12/19/2014	7.98	CENTURY LINK SPLIT	104228 - TELEPHONE
CENTURY LINK	24734	338312172014	12/19/2014	7.98	CENTURY LINK SPLIT	104128 - TELEPHONE
CENTURY LINK	24734	338312172014	12/19/2014	9.98	CENTURY LINK SPLIT	105928 - TELEPHONE
CENTURY LINK	24734	338312172014	12/19/2014	9.98	CENTURY LINK SPLIT	554028 - TELEPHONE
CENTURY LINK	24734	338312172014	12/19/2014	14.97	CENTURY LINK SPLIT	574028 - TELEPHONE
CENTURY LINK	24734	338312172014	12/19/2014	14.97	CENTURY LINK SPLIT	524028 - TELEPHONE
CENTURY LINK	24734	338312172014	12/19/2014	19.96	CENTURY LINK SPLIT	544028 - TELEPHONE
CENTURY LINK	24734	338312172014	12/19/2014	19.96	CENTURY LINK SPLIT	104328 - TELEPHONE
CENTURY LINK	24734	338312172014	12/19/2014	29.94	CENTURY LINK SPLIT	105428 - TELEPHONE
CENTURY LINK	24734	338312172014	12/19/2014	39.92	CENTURY LINK SPLIT	514028 - TELEPHONE
CENTURY LINK	24734	846112162014	12/19/2014	1.38	CENTURY LINK SPLIT	534028 - TELEPHONE
CENTURY LINK	24734	846112162014	12/19/2014	1.38	CENTURY LINK SPLIT	104228 - TELEPHONE
CENTURY LINK	24734	846112162014	12/19/2014	1.38	CENTURY LINK SPLIT	105728 - TELEPHONE
CENTURY LINK	24734	846112162014	12/19/2014	1.38	CENTURY LINK SPLIT	105828 - TELEPHONE
CENTURY LINK	24734	846112162014	12/19/2014	1.38	CENTURY LINK SPLIT	106928 - TELEPHONE
CENTURY LINK	24734	846112162014	12/19/2014	1.38	CENTURY LINK SPLIT	107128 - TELEPHONE
CENTURY LINK	24734	846112162014	12/19/2014	2.76	CENTURY LINK SPLIT	108028 - TELEPHONE
CENTURY LINK	24734	846112162014	12/19/2014	2.76	CENTURY LINK SPLIT	104128 - TELEPHONE
CENTURY LINK	24734	846112162014	12/19/2014	3.45	CENTURY LINK SPLIT	105928 - TELEPHONE
CENTURY LINK	24734	846112162014	12/19/2014	3.45	CENTURY LINK SPLIT	554028 - TELEPHONE
CENTURY LINK	24734	846112162014	12/19/2014	5.18	CENTURY LINK SPLIT	574028 - TELEPHONE
CENTURY LINK	24734	846112162014	12/19/2014	5.18	CENTURY LINK SPLIT	524028 - TELEPHONE
CENTURY LINK	24734	846112162014	12/19/2014		CENTURY LINK SPLIT	544028 - TELEPHONE

Parowan City
Check Register
General Checking - 12/09/2014 to 01/06/2015

Payee Name	Reference Number	Invoice Number	Payment Date	Amount	Description	Ledger Account
CENTURY LINK	24734	846112162014	12/19/2014	6.89	CENTURY LINK SPLIT	104328 - TELEPHONE
CENTURY LINK	24734	846112162014	12/19/2014	6.90	CENTURY LINK SPLIT	105428 - TELEPHONE
CENTURY LINK	24734	846112162014	12/19/2014	10.35	CENTURY LINK SPLIT	514028 - TELEPHONE
CENTURY LINK	24734	846112162014	12/19/2014	13.80	CENTURY LINK SPLIT	534028 - TELEPHONE
CENTURY LINK	24734	910912172014	12/19/2014	1.83	CENTURY LINK SPLIT	104228 - TELEPHONE
CENTURY LINK	24734	910912172014	12/19/2014	1.83	CENTURY LINK SPLIT	105728 - TELEPHONE
CENTURY LINK	24734	910912172014	12/19/2014	1.83	CENTURY LINK SPLIT	106928 - TELEPHONE
CENTURY LINK	24734	910912172014	12/19/2014	1.83	CENTURY LINK SPLIT	107128 - TELEPHONE
CENTURY LINK	24734	910912172014	12/19/2014	1.83	CENTURY LINK SPLIT	108028 - TELEPHONE
CENTURY LINK	24734	910912172014	12/19/2014	1.84	CENTURY LINK SPLIT	105828 - TELEPHONE
CENTURY LINK	24734	910912172014	12/19/2014	3.66	CENTURY LINK SPLIT	104128 - TELEPHONE
CENTURY LINK	24734	910912172014	12/19/2014	3.66	CENTURY LINK SPLIT	105928 - TELEPHONE
CENTURY LINK	24734	910912172014	12/19/2014	4.58	CENTURY LINK SPLIT	554028 - TELEPHONE
CENTURY LINK	24734	910912172014	12/19/2014	4.58	CENTURY LINK SPLIT	574028 - TELEPHONE
CENTURY LINK	24734	910912172014	12/19/2014	6.86	CENTURY LINK SPLIT	524028 - TELEPHONE
CENTURY LINK	24734	910912172014	12/19/2014	6.86	CENTURY LINK SPLIT	544028 - TELEPHONE
CENTURY LINK	24734	910912172014	12/19/2014	9.15	CENTURY LINK SPLIT	104328 - TELEPHONE
CENTURY LINK	24734	910912172014	12/19/2014	9.15	CENTURY LINK SPLIT	105428 - TELEPHONE
CENTURY LINK	24734	910912172014	12/19/2014	13.73	CENTURY LINK SPLIT	514028 - TELEPHONE
CENTURY LINK	24734	910912172014	12/19/2014	18.31	CENTURY LINK SPLIT	534028 - TELEPHONE
				\$360.15		
Child Support Services	24757	PR121214-4256	12/19/2014	535.38	Child Support Services	102245 - MISC/PAYROLL PAYAB
Child Support Services	24776	PR122614-4256	12/31/2014	535.38	Child Support Services	102245 - MISC/PAYROLL PAYAB
				\$1,070.76		
CODALE ELECTRIC SUPPLY, INC	24735	S5260681.001	12/19/2014	174.20	CONDUIT 2-IN IMC	534026 - MAINTENANCE MATERI
COLONIAL LIFE	24694	9813098-121088	12/12/2014	529.03	insurance premium	102252 - COLONIAL INSURANCE
COLOR COUNTRY DIESEL	24695	211950	12/12/2014	290.62	'07 Condor - exhaust manifold	554025 - REPAIR TO EQUIPMEN
COLOR COUNTRY DIESEL	24695	212001	12/12/2014	117.66	'07 Condor - infield fuel hose repair	554025 - REPAIR TO EQUIPMEN
COLOR COUNTRY DIESEL	24695	212301	12/12/2014	131.25	'07 CONDOR EXHAUST PARTS	554025 - REPAIR TO EQUIPMEN
				\$539.53		
DEGEORGE, ANGELO AND LAURA	24761	100000192.1215	12/23/2014	250.00	Deposit Refund: 100000192 - DEGEORGE, ANG	532135 - CUSTOMER DEPOSITS
DEGEORGE, ANGELO AND LAURA	24761	Refund: 1000001	12/23/2014	58.92	Refund: 100000192 - DEGEORGE, ANGELO AN	531311 - ACCOUNTS RECEIVAB
				\$308.92		
DEPARTMENT OF COMMERCE/DI	24696	12092014	12/12/2014	18.00	PAWNSHOP/SECOND HAND MERCHANT TRA	105433 - EDUCATION AND TRAI
DONALD G LANDES	24697	12082014	12/12/2014	298.00	TRAVEL EXPENSE REIMBURSEMENT FOR UA	534023 - TRAVEL, MEALS AND L
DUDLEY, PAT	24698	12012014	12/12/2014	45.00	REFUND FOR CHRISTMAS IN THE COUNTRY	107255 - CHRISTMAS IN THE CO
ENVIRONMENTAL RESTORATION	24762	100000420.1223	12/23/2014	9.40	Deposit Refund: 100000420 - ENVIRONMENTAL	532135 - CUSTOMER DEPOSITS
FADS	24699	12122014	12/12/2014	407.55	STORAGE SHED FOR CEMETERY	108026 - MAINTENANCE MATERI
FASTENAL	24736	UTCED55486	12/19/2014	36.28	SHOP SPLIT	106126 - MAINTENANCE, MATER
FASTENAL	24736	UTCED55486	12/19/2014	36.28	SHOP SPLIT	514026 - MAINTENANCE MATERI
FASTENAL	24736	UTCED55486	12/19/2014	36.28	SHOP SPLIT	524026 - MAINTENANCE MATERI
FASTENAL	24736	UTCED55486	12/19/2014	36.28	SHOP SPLIT	544026 - MAINTENANCE MATERI

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FASTENAL	24736	UTCED55486	12/19/2014	36.28	SHOP SPLIT	574026 - MAINTENANCE MATERI
FASTENAL	24736	UTCED55486	12/19/2014	36.36	SHOP SPLIT	534026 - MAINTENANCE MATERI
				\$217.76		
HALIGARDA, WILLIAM	24737	Refund: 9001390	12/19/2014	42.30	Refund: 900139002 - HALIGARDA, WILLIAM	531311 - ACCOUNTS RECEIVAB
HAMBURGER PATTY'S	24700	1425	12/12/2014	523.25	CHRISTMAS PARTY DINNER 2014 FOR 70 PEO	104360 - EMPLOYEE LOUNGE
HAMBURGER PATTY'S	24700	1425	12/12/2014	523.25	CHRISTMAS PARTY DINNER 2014 FOR 70 PEO	107268 - SPECIAL CELEBRATIO
				\$1,046.50		
HEALTH EQUITY	12181401	PR121214-4720	12/23/2014	250.00	HSA Savings Account EE	102249 - HEALTH SAVINGS ACC
HEALTH EQUITY	12181401	PR121214-4720	12/23/2014	800.00	HSA Savings Account	102249 - HEALTH SAVINGS ACC
HEALTH EQUITY	12311401	PR122614-4720	12/30/2014	250.00	HSA Savings Account EE	102249 - HEALTH SAVINGS ACC
				\$1,300.00		
HERO PLUMBING, LLC	24701	DEC-10008	12/12/2014	70.00	PORTABLE TOILET RENTAL - DEC 2014	107026 - MAINTENANCE MATERI
HOME DEPOT CREDIT SERVICES	24702	1014576	12/12/2014	68.64	SHOP SPLIT	514026 - MAINTENANCE MATERI
HOME DEPOT CREDIT SERVICES	24702	1014576	12/12/2014	68.64	SHOP SPLIT	524026 - MAINTENANCE MATERI
HOME DEPOT CREDIT SERVICES	24702	1014576	12/12/2014	68.64	SHOP SPLIT	544026 - MAINTENANCE MATERI
HOME DEPOT CREDIT SERVICES	24702	1014576	12/12/2014	68.64	SHOP SPLIT	574026 - MAINTENANCE MATERI
HOME DEPOT CREDIT SERVICES	24702	1014576	12/12/2014	68.66	SHOP SPLIT	106126 - MAINTENANCE, MATER
HOME DEPOT CREDIT SERVICES	24702	1014576	12/12/2014	68.80	SHOP SPLIT	534026 - MAINTENANCE MATERI
HOME DEPOT CREDIT SERVICES	24702	5560038	12/12/2014	70.59	SHOP SPLIT	106126 - MAINTENANCE, MATER
HOME DEPOT CREDIT SERVICES	24702	5560038	12/12/2014	70.59	SHOP SPLIT	514026 - MAINTENANCE MATERI
HOME DEPOT CREDIT SERVICES	24702	5560038	12/12/2014	70.59	SHOP SPLIT	524026 - MAINTENANCE MATERI
HOME DEPOT CREDIT SERVICES	24702	5560038	12/12/2014	70.59	SHOP SPLIT	544026 - MAINTENANCE MATERI
HOME DEPOT CREDIT SERVICES	24702	5560038	12/12/2014	70.59	SHOP SPLIT	574026 - MAINTENANCE MATERI
HOME DEPOT CREDIT SERVICES	24702	5560038	12/12/2014	70.76	SHOP SPLIT	534026 - MAINTENANCE MATERI
				\$835.73		
IMAGE PRO	24738	72307	12/19/2014	50.32	PAROWAN'S BIRTHDAY POSTER 2015 - JET S	107268 - SPECIAL CELEBRATIO
INGRAM LIBRARY SERVICES	24739	81787981	12/19/2014	37.74	BOOKS	107529 - CLEF GRANT EXPENDI
IRON GATE BUILDERS	24763	2130	12/23/2014	8,172.00	FINAL PAYMENT ON MAIN STREET BATHROO	444031 - ENGINEERING
JENSEN, RICHARD	24764	100000296.1223	12/23/2014	142.94	Deposit Refund: 100000296 - JENSEN, RICHAR	532135 - CUSTOMER DEPOSITS
L & W SERVICES OF PAROWAN IN	24703	9248	12/12/2014	63.19	CRIMP COUPLING, WHO6U-108, HYD HOSE, M	107126 - MAINTENANCE MATERI
L & W SERVICES OF PAROWAN IN	24703	9249	12/12/2014	29.02	HITCH PIN- FOR SNOW PLOW	106125 - REPAIR TO EQUIPMEN
L & W SERVICES OF PAROWAN IN	24703	9255	12/12/2014	18.26	PARTS TO REPAIR '04 CONDOR	554025 - REPAIR TO EQUIPMEN
				\$110.47		
LegalShield	24777	PR121214-3755	12/31/2014	12.95	Pre-Paid Legal	102245 - MISC/PAYROLL PAYAB
LegalShield	24777	PR122614-3755	12/31/2014	12.95	Pre-Paid Legal	102245 - MISC/PAYROLL PAYAB
				\$25.90		
LONG TERM DISABILITY PROGRA	24778	PR112814-354	12/31/2014	285.15	Long Term Disability	102230 - RETIREMENT PAYABLE
LONG TERM DISABILITY PROGRA	24778	PR121214-354	12/31/2014	283.98	Long Term Disability	102230 - RETIREMENT PAYABLE
				\$569.13		
MATHESON, DAVID H	24766	12222014	12/23/2014	48.29	RESTITUTION PAID BY CORTNEY STEVENS F	104235 - RESTITUTION

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MICROMARKETING ASSOCIATES	24704	551041	12/12/2014	37.74	books	107529 - CLEF GRANT EXPENDI
MICROMARKETING ASSOCIATES	24704	551520	12/12/2014	81.00	books on CD	107529 - CLEF GRANT EXPENDI
				\$118.74		
MONSTER INK & DESIGN	24740	1119	12/19/2014	14.00	REFLECTIVE VINYL FOR POLICE JACKETS	105447 - UNIFORM ALLOWANCE
MOUNT OLYMPUS WATERS, INC	24741	10217034 12051	12/19/2014	22.24	WATER COOLER RENTAL & 1 WATER	534061 - SUNDRY
MOUNT OLYMPUS WATERS, INC	24741	10220009 12051	12/19/2014	24.89	WATER COOLER RENTAL & 2 WATER	514061 - SUNDRY
MOUNT OLYMPUS WATERS, INC	24741	10222960 12091	12/19/2014	39.67	WATER COOLER RENTAL	104361 - SUNDRY
				\$86.80		
Mountain America Credit Union	24758	PR121214-3752	12/19/2014	545.00	Credit Union	102240 - CREDIT UNION PAYAB
Mountain America Credit Union	24779	PR122614-3752	12/31/2014	545.00	Credit Union	102240 - CREDIT UNION PAYAB
				\$1,090.00		
MOUNTAIN WEST COMPUTERS	24767	43630	12/23/2014	313.00	ACER LCD DISPLAY MONITOR - KELLY - WATE	514024 - OFFICE SUPPLIES AND
OMS	24705	12082014	12/12/2014	30.00	restitution from Bryant Coburn case # 095201061	104235 - RESTITUTION
OMS	24768	12222014	12/23/2014	30.00	RESTITUTION FROM BRYANT COBURN CASE#	104235 - RESTITUTION
				\$60.00		
PACE'S CULLIGAN BOTTLED WAT	24706	69408	12/12/2014	9.95	water cooler rental	104261 - SUNDRY
PAGE, SILVIA	24707	12082014	12/12/2014	37.50	interpreter fees for case # 145200388	104231 - PROFESSIONAL AND T
PAROWAN MARKET INC.	24708	051742	12/12/2014	25.97	drinks for fire fighters	105723 - TRAVEL, MEALS AND L
PAROWAN MARKET INC.	24708	091415	12/12/2014	14.69	gift wrap for Christmas in the Country	107255 - CHRISTMAS IN THE CO
PAROWAN MARKET INC.	24708	261441	12/12/2014	12.28	parade supplies	107254 - PARADES
				\$52.94		
PAROWAN TREASURER	24742	122012172014	12/19/2014	89.68	HERITAGE PARK	107027 - UTILITIES
PAROWAN TREASURER	24742	318312172014	12/19/2014	227.94	FIRE DEPARTMENT	105727 - UTILITIES
PAROWAN TREASURER	24742	318412172014	12/19/2014	26.73	PIONEER INDUSTRIAL PARK	106227 - UTILITIES
PAROWAN TREASURER	24742	410412172014	12/19/2014	55.16	AIRPORT RESTROOMS	108527 - UTILITIES & MISCELLA
PAROWAN TREASURER	24742	4105012172014	12/19/2014	150.12	AIRPORT RUNWAY LIGHTS	108527 - UTILITIES & MISCELLA
PAROWAN TREASURER	24742	410512172014	12/19/2014	178.09	DOG POUND	105527 - UTILITIES
PAROWAN TREASURER	24742	411612172014	12/19/2014	22.23	405 N MAIN	514027 - UTILITIES
PAROWAN TREASURER	24742	4117012172014	12/19/2014	193.56	405 N MAIN	108527 - UTILITIES & MISCELLA
PAROWAN TREASURER	24742	411712172014	12/19/2014	80.63	WATER SHOP	514027 - UTILITIES
PAROWAN TREASURER	24742	411812172014	12/19/2014	22.67	UTILITY SPLIT	524027 - UTILITIES
PAROWAN TREASURER	24742	411812172014	12/19/2014	22.67	UTILITY SPLIT	574027 - UTILITIES
PAROWAN TREASURER	24742	411812172014	12/19/2014	45.28	UTILITY SPLIT	544027 - UTILITIES
PAROWAN TREASURER	24742	411812172014	12/19/2014	90.59	UTILITY SPLIT	514027 - UTILITIES
PAROWAN TREASURER	24742	411812172014	12/19/2014	90.59	UTILITY SPLIT	534027 - UTILITIES
PAROWAN TREASURER	24742	4159012172014	12/19/2014	163.99	73 N MAIN	105927 - UTILITIES
PAROWAN TREASURER	24742	415912172014	12/19/2014	78.92	VISITORS CENTER	105927 - UTILITIES
PAROWAN TREASURER	24742	4159212172014	12/19/2014	65.27	VISITORS CENTER #3	105927 - UTILITIES
PAROWAN TREASURER	24742	419700001	12/19/2014	22.18	UTILITY SPLIT	524027 - UTILITIES
PAROWAN TREASURER	24742	419700001	12/19/2014	22.18	UTILITY SPLIT	574027 - UTILITIES
PAROWAN TREASURER	24742	419700001	12/19/2014	44.30	UTILITY SPLIT	544027 - UTILITIES
PAROWAN TREASURER	24742	419700001	12/19/2014	88.63	UTILITY SPLIT	514027 - UTILITIES
PAROWAN TREASURER	24742	419700001	12/19/2014	88.63	UTILITY SPLIT	534027 - UTILITIES

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PAROWAN TREASURER	24742	419812172014	12/19/2014	459.06	LIBRARY	107527 - UTILITIES
PAROWAN TREASURER	24742	421012172014	12/19/2014	219.33	THEATER	107327 - UTILITIES
PAROWAN TREASURER	24742	422612172014	12/19/2014	82.58	DUP - OLD ROCK CHURCH	104927 - UTILITIES
PAROWAN TREASURER	24742	423012172014	12/19/2014	86.02	JESSE SMITH HOME	104927 - UTILITIES
PAROWAN TREASURER	24742	6100012172014	12/19/2014	28.76	LIONS PAVILLION	107027 - UTILITIES
PAROWAN TREASURER	24742	610012172014	12/19/2014	99.97	SWIMMING POOL	106927 - UTILITIES
PAROWAN TREASURER	24742	614712172014	12/19/2014	46.10	CEMETERY WATER	108027 - UTILITIES
PAROWAN TREASURER	24742	614912172014	12/19/2014	1,341.37	MAIN CANYON	514027 - UTILITIES
PAROWAN TREASURER	24742	615412172014	12/19/2014	100.63	CITY CHLORINATOR	574027 - UTILITIES
PAROWAN TREASURER	24742	618912172014	12/19/2014	17.92	RODEO GROUNDS	107127 - UTILITIES
PAROWAN TREASURER	24742	6189701217201	12/19/2014	323.64	EXB BUILDING	107127 - UTILITIES
PAROWAN TREASURER	24742	6189712172014	12/19/2014	1.48	SOCCER FIELD	107027 - UTILITIES
PAROWAN TREASURER	24742	6189721217201	12/19/2014	122.36	FAIR GR CON	107127 - UTILITIES
PAROWAN TREASURER	24742	6189741217201	12/19/2014	19.51	BBALL FIELD	107027 - UTILITIES
PAROWAN TREASURER	24742	620012172014	12/19/2014	52.02	RACE TRACK WELL	574027 - UTILITIES
PAROWAN TREASURER	24742	751312172014	12/19/2014	10.75	WEEKS POND	107027 - UTILITIES
PAROWAN TREASURER	24742	751812172014	12/19/2014	10.75	POWER PLANT	534027 - UTILITIES
PAROWAN TREASURER	24742	760412172014	12/19/2014	10.75	PI 100 S & MAIN	107027 - UTILITIES
PAROWAN TREASURER	24742	760512172014	12/19/2014	10.75	CHURCH SQUARE	107027 - UTILITIES
				\$4,913.79		
POSTMASTER	24781	01022015	01/02/2015	15.10	POSTAGE SPLIT	107048 - POSTAGE
POSTMASTER	24781	01022015	01/02/2015	30.20	POSTAGE SPLIT	104348 - POSTAGE
POSTMASTER	24781	01022015	01/02/2015	30.20	POSTAGE SPLIT	105948 - POSTAGE
POSTMASTER	24781	01022015	01/02/2015	67.95	POSTAGE SPLIT	524048 - POSTAGE
POSTMASTER	24781	01022015	01/02/2015	67.95	POSTAGE SPLIT	544048 - POSTAGE
POSTMASTER	24781	01022015	01/02/2015	135.90	POSTAGE SPLIT	514048 - POSTAGE
POSTMASTER	24781	01022015	01/02/2015	135.90	POSTAGE SPLIT	534048 - POSTAGE
POSTMASTER	24781	01022015	01/02/2015	135.90	POSTAGE SPLIT	554048 - POSTAGE
POSTMASTER	24781	01022015	01/02/2015	135.90	POSTAGE SPLIT	574048 - POSTAGE
POSTMASTER	24782	01052014	01/05/2015	15.10	POSTAGE SPLIT	107048 - POSTAGE
POSTMASTER	24782	01052014	01/05/2015	30.20	POSTAGE SPLIT	104348 - POSTAGE
POSTMASTER	24782	01052014	01/05/2015	30.20	POSTAGE SPLIT	105948 - POSTAGE
POSTMASTER	24782	01052014	01/05/2015	67.95	POSTAGE SPLIT	524048 - POSTAGE
POSTMASTER	24782	01052014	01/05/2015	67.95	POSTAGE SPLIT	544048 - POSTAGE
POSTMASTER	24782	01052014	01/05/2015	135.90	POSTAGE SPLIT	514048 - POSTAGE
POSTMASTER	24782	01052014	01/05/2015	135.90	POSTAGE SPLIT	534048 - POSTAGE
POSTMASTER	24782	01052014	01/05/2015	135.90	POSTAGE SPLIT	554048 - POSTAGE
POSTMASTER	24782	01052014	01/05/2015	135.90	POSTAGE SPLIT	574048 - POSTAGE
				\$1,510.00		
PRATT, DIANE	24743	12162014	12/19/2014	50.00	REFUND FOR MARATHON - DIANE IS AN EMPL	107263 - MARATHONS/RACES
PUBLIC EMPLOYEES HEALTH PR	24769	0121397132	12/23/2014	28,370.16	DECEMBER HEALTH COVERAGE/DENTAL CO	102250 - HEALTH INSURANCE P
PUBLIC EMPLOYEES HEALTH PR	24769	121397133	12/23/2014	2,324.15	DECEMBER HEALTH COVERAGE/DENTAL CO	102250 - HEALTH INSURANCE P
				\$30,694.31		
QUESTAR GAS	24709	008612082014	12/12/2014	231.09	15 S MAIN	534027 - UTILITIES
QUESTAR GAS	24709	223312082014	12/12/2014	13.72	33 W 100 S	104927 - UTILITIES
QUESTAR GAS	24709	298712082014	12/12/2014	138.69	405 N MAIN	534027 - UTILITIES

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QUESTAR GAS	24709	320312082014	12/12/2014	115.44	73 N MAIN	105927 - UTILITIES
QUESTAR GAS	24709	325312082014	12/12/2014	776.63	117 S 550 E	107127 - UTILITIES
QUESTAR GAS	24709	405612082014	12/12/2014	21.08	50 W CENTER	104927 - UTILITIES
QUESTAR GAS	24709	489212082014	12/12/2014	194.18	27 N MAIN	107327 - UTILITIES
QUESTAR GAS	24709	543612082014	12/12/2014	19.35	89 S 300 E	106927 - UTILITIES
QUESTAR GAS	24709	922512082014	12/12/2014	113.79	160 W 200 S	105727 - UTILITIES
QUESTAR GAS	24709	958812082014	12/12/2014	158.07	5 S MAIN	544027 - UTILITIES
				\$1,782.04		
RANDALL C ALLEN	24710	12112014	12/12/2014	2,400.00	PUBLIC DEFENDER FEES FOR JANUARY 2014	104231 - PROFESSIONAL AND T
ROCKY MOUNTAIN INFO NETWORK	24711	12092014	12/12/2014	50.00	FY 2014-15 ANNUAL MEMBERSHIP FEES	105421 - SUBSCRIPTIONS AND
ROCKY MOUNTAIN POWER	24744	80014 12162014	12/19/2014	318.14	2600 N 2600 W	544027 - UTILITIES
ROCKY MOUNTAIN POWER	24744	8003 12162014	12/19/2014	115.66	2650 W 2600 N	544027 - UTILITIES
				\$433.80		
ROSENBERG ASSOCIATES	24745	10159	12/19/2014	125.00	ALLISON LOT LINE ADJUSTMENT PROJECT# 6	105826 - MAINTENANCE MATERI
RUDDER, HOLLIE	24746	12172014	12/19/2014	1,833.00	CASH BAIL REFUND THAT WAS POSTED FOR	104237 - BAIL
SALT LAKE WHOLESALE SPORTS	24712	26196	12/12/2014	3,750.00	dedicated simunition training weapons	105449 - SPECIAL DEPARTMEN
SCHOLZEN PRODUCTS	24713	6001950-00	12/12/2014	3,890.82	meter supplies for KB Xpress	514026 - MAINTENANCE MATERI
SCHOLZEN PRODUCTS	24747	HR03000372	12/19/2014	105.60	CYLINDER RENT	514026 - MAINTENANCE MATERI
				\$3,996.42		
SOUTH CENTRAL COMMUNICATI	24748	DEC 2014 I	12/19/2014	6.40	TELEPHONE SPLIT	104228 - TELEPHONE
SOUTH CENTRAL COMMUNICATI	24748	DEC 2014 I	12/19/2014	6.40	TELEPHONE SPLIT	105728 - TELEPHONE
SOUTH CENTRAL COMMUNICATI	24748	DEC 2014 I	12/19/2014	6.40	TELEPHONE SPLIT	105828 - TELEPHONE
SOUTH CENTRAL COMMUNICATI	24748	DEC 2014 I	12/19/2014	6.40	TELEPHONE SPLIT	106928 - TELEPHONE
SOUTH CENTRAL COMMUNICATI	24748	DEC 2014 I	12/19/2014	6.40	TELEPHONE SPLIT	107128 - TELEPHONE
SOUTH CENTRAL COMMUNICATI	24748	DEC 2014 I	12/19/2014	6.40	TELEPHONE SPLIT	108028 - TELEPHONE
SOUTH CENTRAL COMMUNICATI	24748	DEC 2014 I	12/19/2014	12.81	TELEPHONE SPLIT	104128 - TELEPHONE
SOUTH CENTRAL COMMUNICATI	24748	DEC 2014 I	12/19/2014	12.81	TELEPHONE SPLIT	105928 - TELEPHONE
SOUTH CENTRAL COMMUNICATI	24748	DEC 2014 I	12/19/2014	16.01	TELEPHONE SPLIT	554028 - TELEPHONE
SOUTH CENTRAL COMMUNICATI	24748	DEC 2014 I	12/19/2014	16.01	TELEPHONE SPLIT	574028 - TELEPHONE
SOUTH CENTRAL COMMUNICATI	24748	DEC 2014 I	12/19/2014	24.01	TELEPHONE SPLIT	524028 - TELEPHONE
SOUTH CENTRAL COMMUNICATI	24748	DEC 2014 I	12/19/2014	24.01	TELEPHONE SPLIT	544028 - TELEPHONE
SOUTH CENTRAL COMMUNICATI	24748	DEC 2014 I	12/19/2014	32.02	TELEPHONE SPLIT	104328 - TELEPHONE
SOUTH CENTRAL COMMUNICATI	24748	DEC 2014 I	12/19/2014	32.02	TELEPHONE SPLIT	105428 - TELEPHONE
SOUTH CENTRAL COMMUNICATI	24748	DEC 2014 I	12/19/2014	48.02	TELEPHONE SPLIT	514028 - TELEPHONE
SOUTH CENTRAL COMMUNICATI	24748	DEC 2014 I	12/19/2014	64.03	TELEPHONE SPLIT	534028 - TELEPHONE
SOUTH CENTRAL COMMUNICATI	24748	DEC 2014 P	12/19/2014	7.32	TELEPHONE SPLIT	105728 - TELEPHONE
SOUTH CENTRAL COMMUNICATI	24748	DEC 2014 P	12/19/2014	7.32	TELEPHONE SPLIT	105828 - TELEPHONE
SOUTH CENTRAL COMMUNICATI	24748	DEC 2014 P	12/19/2014	7.32	TELEPHONE SPLIT	106928 - TELEPHONE
SOUTH CENTRAL COMMUNICATI	24748	DEC 2014 P	12/19/2014	7.32	TELEPHONE SPLIT	107128 - TELEPHONE
SOUTH CENTRAL COMMUNICATI	24748	DEC 2014 P	12/19/2014	7.32	TELEPHONE SPLIT	108028 - TELEPHONE
SOUTH CENTRAL COMMUNICATI	24748	DEC 2014 P	12/19/2014	7.33	TELEPHONE SPLIT	104228 - TELEPHONE
SOUTH CENTRAL COMMUNICATI	24748	DEC 2014 P	12/19/2014	14.65	TELEPHONE SPLIT	104128 - TELEPHONE
SOUTH CENTRAL COMMUNICATI	24748	DEC 2014 P	12/19/2014	14.65	TELEPHONE SPLIT	105928 - TELEPHONE
SOUTH CENTRAL COMMUNICATI	24748	DEC 2014 P	12/19/2014	18.31	TELEPHONE SPLIT	554028 - TELEPHONE

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SOUTH CENTRAL COMMUNICATI	24748	DEC 2014 P	12/19/2014	18.31	TELEPHONE SPLIT	574028 - TELEPHONE
SOUTH CENTRAL COMMUNICATI	24748	DEC 2014 P	12/19/2014	27.46	TELEPHONE SPLIT	524028 - TELEPHONE
SOUTH CENTRAL COMMUNICATI	24748	DEC 2014 P	12/19/2014	27.46	TELEPHONE SPLIT	544028 - TELEPHONE
SOUTH CENTRAL COMMUNICATI	24748	DEC 2014 P	12/19/2014	36.62	TELEPHONE SPLIT	104328 - TELEPHONE
SOUTH CENTRAL COMMUNICATI	24748	DEC 2014 P	12/19/2014	36.62	TELEPHONE SPLIT	105428 - TELEPHONE
SOUTH CENTRAL COMMUNICATI	24748	DEC 2014 P	12/19/2014	54.92	TELEPHONE SPLIT	514028 - TELEPHONE
SOUTH CENTRAL COMMUNICATI	24748	DEC 2014 P	12/19/2014	73.23	TELEPHONESPLIT	534028 - TELEPHONE
				\$686.31		
SOUTHERN UTAH OFFICE MACHI	24714	65683	12/12/2014	17.75	sticky notes and envelopes	104224 - OFFICE SUPPLIES AND
SOUTHERN UTAH OFFICE MACHI	24749	65636	12/19/2014	126.16	repairs on Savin copier - Library	107526 - MAINTENANCE MATERI
SOUTHERN UTAH OFFICE MACHI	24770	65909	12/23/2014	42.00	COPY PAPER	105926 - MAINTENANCE MATERI
				\$185.91		
SOUTHERN UTAH UNIVERSITY	24715	S0028649	12/12/2014	40.00	WATER LABS K201402580 & K201402581	514031 - PROFESSIONAL & TEC
STATE BANK OF SOUTHERN UTA	24750	158212162014	12/19/2014	8.98	PUBLIC WORKS SPLIT	514026 - MAINTENANCE MATERI
STATE BANK OF SOUTHERN UTA	24750	158212162014	12/19/2014	9.00	PUBLIC WORKS SPLIT	524026 - MAINTENANCE MATERI
STATE BANK OF SOUTHERN UTA	24750	158212162014	12/19/2014	9.00	PUBLIC WORKS SPLIT	106126 - MAINTENANCE MATERI
STATE BANK OF SOUTHERN UTA	24750	158212162014	12/19/2014	9.00	PUBLIC WORKS SPLIT	544026 - MAINTENANCE MATERI
STATE BANK OF SOUTHERN UTA	24750	158212162014	12/19/2014	9.00	PUBLIC WORKS SPLIT	574026 - MAINTENANCE MATERI
STATE BANK OF SOUTHERN UTA	24750	159012162014-1	12/19/2014	23.84	POSTAGE SPLIT	107048 - POSTAGE
STATE BANK OF SOUTHERN UTA	24750	159012162014-1	12/19/2014	47.67	POSTAGE SPLIT	105948 - POSTAGE
STATE BANK OF SOUTHERN UTA	24750	159012162014-1	12/19/2014	47.68	POSTAGE SPLIT	104348 - POSTAGE
STATE BANK OF SOUTHERN UTA	24750	159012162014-1	12/19/2014	107.27	POSTAGE SPLIT	524048 - POSTAGE
STATE BANK OF SOUTHERN UTA	24750	159012162014-1	12/19/2014	107.27	POSTAGE SPLIT	544048 - POSTAGE
STATE BANK OF SOUTHERN UTA	24750	159012162014-1	12/19/2014	214.53	POSTAGE SPLIT	514048 - POSTAGE
STATE BANK OF SOUTHERN UTA	24750	159012162014-1	12/19/2014	214.53	POSTAGE SPLIT	534048 - POSTAGE
STATE BANK OF SOUTHERN UTA	24750	159012162014-1	12/19/2014	214.53	POSTAGE SPLIT	554048 - POSTAGE
STATE BANK OF SOUTHERN UTA	24750	159012162014-1	12/19/2014	214.53	POSTAGE SPLIT	574048 - POSTAGE
STATE BANK OF SOUTHERN UTA	24750	159012162014-2	12/19/2014	0.60	OFFICE SUPPLIES SPLIT	104224 - OFFICE SUPPLIES AND
STATE BANK OF SOUTHERN UTA	24750	159012162014-2	12/19/2014	0.60	OFFICE SUPPLIES SPLIT	105824 - OFFICE SUPPLIES AND
STATE BANK OF SOUTHERN UTA	24750	159012162014-2	12/19/2014	0.60	OFFICE SUPPLIES SPLIT	106926 - MAINTENANCE MATERI
STATE BANK OF SOUTHERN UTA	24750	159012162014-2	12/19/2014	0.60	OFFICE SUPPLIES SPLIT	108026 - MAINTENANCE MATERI
STATE BANK OF SOUTHERN UTA	24750	159012162014-2	12/19/2014	1.20	OFFICE SUPPLIES SPLIT	104124 - OFFICE SUPPLIES AND
STATE BANK OF SOUTHERN UTA	24750	159012162014-2	12/19/2014	1.20	OFFICE SUPPLIES SPLIT	105926 - MAINTENANCE MATERI
STATE BANK OF SOUTHERN UTA	24750	159012162014-2	12/19/2014	2.96	OFFICE SUPPLIES SPLIT	105424 - OFFICE SUPPLIES AND
STATE BANK OF SOUTHERN UTA	24750	159012162014-2	12/19/2014	4.19	OFFICE SUPPLIES SPLIT	104324 - OFFICE SUPPLIES AND
STATE BANK OF SOUTHERN UTA	24750	159012162014-2	12/19/2014	5.67	OFFICE SUPPLIES SPLIT	524024 - OFFICE SUPPLIES AND
STATE BANK OF SOUTHERN UTA	24750	159012162014-2	12/19/2014	5.67	OFFICE SUPPLIES SPLIT	544024 - OFFICE SUPPLIES AND
STATE BANK OF SOUTHERN UTA	24750	159012162014-2	12/19/2014	6.30	OFFICE SUPPLIES SPLIT	574026 - MAINTENANCE MATERI
STATE BANK OF SOUTHERN UTA	24750	159012162014-2	12/19/2014	9.45	OFFICE SUPPLIES SPLIT	554024 - OFFICE SUPPLIES AND
STATE BANK OF SOUTHERN UTA	24750	159012162014-2	12/19/2014	11.34	OFFICE SUPPLIES SPLIT	514024 - OFFICE SUPPLIES AND
STATE BANK OF SOUTHERN UTA	24750	159012162014-2	12/19/2014	12.60	OFFICE SUPPLIES SPLIT	534024 - OFFICE SUPPLIES AND
STATE BANK OF SOUTHERN UTA	24750	159012162014-2	12/19/2014	23.70	NVIDIA TRAVEL SALEM, VA	105447 - UNIFORM ALLOWANCE
STATE BANK OF SOUTHERN UTA	24750	162412162014	12/19/2014	26.72	ACP CONFERENCE - DOUBLETREE SUITES -	102138 - SALES TAX PAYABLE
STATE BANK OF SOUTHERN UTA	24750	162412162014	12/19/2014	35.45	ACP CONFERENCE - HAMPTON INN CONV CE	102138 - SALES TAX PAYABLE
STATE BANK OF SOUTHERN UTA	24750	162412162014	12/19/2014	49.49	ACP CONFERENCE - RUBY TUESDAY - FLORI	105423 - TRAVEL, MEALS AND L
STATE BANK OF SOUTHERN UTA	24750	162412162014	12/19/2014	50.00	ACP CONFERENCE - BAGGAGE CHARGE	105423 - TRAVEL, MEALS AND L

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STATE BANK OF SOUTHERN UTA	24750	162412162014	12/19/2014	54.00	IACP CONFERENCE - PARKING, SLC AIRPORT	105423 - TRAVEL, MEALS AND L
STATE BANK OF SOUTHERN UTA	24750	162412162014	12/19/2014	61.73	IACP CONFERENCE - STEPHEN'S AMERICAN	105423 - TRAVEL, MEALS AND L
STATE BANK OF SOUTHERN UTA	24750	162412162014	12/19/2014	71.16	IACP CONFERENCE - LONGHORN STEAK HOU	105423 - TRAVEL, MEALS AND L
STATE BANK OF SOUTHERN UTA	24750	162412162014	12/19/2014	212.00	IACP CONFERENCE - DOUBLETREE SUITES -	105423 - TRAVEL, MEALS AND L
STATE BANK OF SOUTHERN UTA	24750	162412162014	12/19/2014	234.92	SKAGGS COMPANIES - UNIFORMS	105447 - UNIFORM ALLOWANCE
STATE BANK OF SOUTHERN UTA	24750	162412162014	12/19/2014	420.50	IACP CONFERENCE - ENTERPRISE RENT-A-C	105423 - TRAVEL, MEALS AND L
STATE BANK OF SOUTHERN UTA	24750	162412162014	12/19/2014	577.70	IACP CONFERENCE - HAMPTON INN CONV CE	105423 - TRAVEL, MEALS AND L
STATE BANK OF SOUTHERN UTA	24750	163212162014	12/19/2014	588.75	POSTAGE FOR JUSTICE COURT	104248 - POSTAGE
STATE BANK OF SOUTHERN UTA	24750	165712162014	12/19/2014	45.50	WALMART - BOOKS	107529 - CLEF GRANT EXPENDI
STATE BANK OF SOUTHERN UTA	24750	165712162014	12/19/2014	86.63	WALMART - BOOKS	107529 - CLEF GRANT EXPENDI
STATE BANK OF SOUTHERN UTA	24750	165712162014	12/19/2014	89.49	TONER FOR MAYOR'S PRINTER	104124 - OFFICE SUPPLIES AND
STATE BANK OF SOUTHERN UTA	24750	165712162014	12/19/2014	638.95	STAPLES	107524 - OFFICE SUPPLIES AND
STATE BANK OF SOUTHERN UTA	24750	166512162014	12/19/2014	17.49	CHRISTMAS TREE FOR CITY OFFICE - SPLIT	534026 - MAINTENANCE MATERI
STATE BANK OF SOUTHERN UTA	24750	166512162014	12/19/2014	17.50	CHRISTMAS TREE FOR CITY OFFICE - SPLIT	104326 - MAINTENANCE MATERI
STATE BANK OF SOUTHERN UTA	24750	166512162014	12/19/2014	17.50	CHRISTMAS TREE FOR CITY OFFICE - SPLIT	514026 - MAINTENANCE MATERI
STATE BANK OF SOUTHERN UTA	24750	166512162014	12/19/2014	17.50	CHRISTMAS TREE FOR CITY OFFICE - SPLIT	524026 - MAINTENANCE MATERI
STATE BANK OF SOUTHERN UTA	24750	166512162014	12/19/2014	17.50	CHRISTMAS TREE FOR CITY OFFICE - SPLIT	544026 - MAINTENANCE MATERI
STATE BANK OF SOUTHERN UTA	24750	166512162014	12/19/2014	17.50	CHRISTMAS TREE FOR CITY OFFICE - SPLIT	574026 - MAINTENANCE MATERI
STATE BANK OF SOUTHERN UTA	24750	166512162014	12/19/2014	40.00	UVU UFRA INTERNET - RECERTIFICATION	105726 - MAINTENANCE MATERI
STATE BANK OF SOUTHERN UTA	24750	166512162014	12/19/2014	120.00	UVU UFRA INTERNET - RECERTIFICATION	105726 - MAINTENANCE MATERI
STATE BANK OF SOUTHERN UTA	24750	166512162014	12/19/2014	154.13	PIZZA BARN - FIRE PARTY	105723 - TRAVEL, MEALS AND L
STATE BANK OF SOUTHERN UTA	24750	168112162014	12/19/2014	54.86	HARBOR FREIGHT TOOLS-SUB FOR SANTA	107252 - SUB FOR SANTA
STATE BANK OF SOUTHERN UTA	24750	168112162014	12/19/2014	103.37	U SAVE CLOSEOUTS&LIQUIDATORS	107252 - SUB FOR SANTA
STATE BANK OF SOUTHERN UTA	24750	168112162014	12/19/2014	166.71	BIG LOTS - SUB FOR SANTA	107252 - SUB FOR SANTA
STATE BANK OF SOUTHERN UTA	24750	168112162014	12/19/2014	399.68	WALMART - SUB FOR SANTA	107252 - SUB FOR SANTA
STATE BANK OF SOUTHERN UTA	24750	301812192014	12/19/2014	27.71	SUPERSONIC CAR WASH - CITY CAR	104323 - TRAVEL, MEALS AND L
STATE BANK OF SOUTHERN UTA	24750	301812192014	12/19/2014	29.29	PAROWAN MARKET	104967 - ECONOMIC DEVELOPM
STATE BANK OF SOUTHERN UTA	24750	389312162014-1	12/19/2014	46.93	FOUR PEAKS BREWING COMPANY - POWER	534023 - TRAVEL, MEALS AND L
STATE BANK OF SOUTHERN UTA	24750	389312162014-1	12/19/2014	4.15	SHOP SPLIT	534026 - MAINTENANCE MATERI
STATE BANK OF SOUTHERN UTA	24750	389312162014-1	12/19/2014	4.16	SHOP SPLIT	106126 - MAINTENANCE MATERI
STATE BANK OF SOUTHERN UTA	24750	389312162014-1	12/19/2014	4.16	SHOP SPLIT	514026 - MAINTENANCE MATERI
STATE BANK OF SOUTHERN UTA	24750	389312162014-1	12/19/2014	4.16	SHOP SPLIT	524026 - MAINTENANCE MATERI
STATE BANK OF SOUTHERN UTA	24750	389312162014-1	12/19/2014	4.16	SHOP SPLIT	544026 - MAINTENANCE MATERI
STATE BANK OF SOUTHERN UTA	24750	389312162014-1	12/19/2014	4.16	SHOP SPLIT	574026 - MAINTENANCE MATERI
STATE BANK OF SOUTHERN UTA	24750	389312162014-2	12/19/2014	5.14	SHOP SPLIT	106126 - MAINTENANCE MATERI
STATE BANK OF SOUTHERN UTA	24750	389312162014-2	12/19/2014	5.14	SHOP SPLIT	514026 - MAINTENANCE MATERI
STATE BANK OF SOUTHERN UTA	24750	389312162014-2	12/19/2014	5.14	SHOP SPLIT	524026 - MAINTENANCE MATERI
STATE BANK OF SOUTHERN UTA	24750	389312162014-2	12/19/2014	5.14	SHOP SPLIT	534026 - MAINTENANCE MATERI
STATE BANK OF SOUTHERN UTA	24750	389312162014-2	12/19/2014	5.14	SHOP SPLIT	544026 - MAINTENANCE MATERI
STATE BANK OF SOUTHERN UTA	24750	389312162014-2	12/19/2014	5.14	SHOP SPLIT	574026 - MAINTENANCE MATERI
STATE BANK OF SOUTHERN UTA	24750	389312162014-2	12/19/2014	48.77	MOTION INDUSTRIES	554025 - REPAIR TO EQUIPMEN
STATE BANK OF SOUTHERN UTA	24750	389312162014-2	12/19/2014	49.98	NEWARKINONE	554025 - REPAIR TO EQUIPMEN
STATE BANK OF SOUTHERN UTA	24750	389312162014-2	12/19/2014	64.75	MOUSER ELECTRONICS	554025 - REPAIR TO EQUIPMEN
STATE BANK OF SOUTHERN UTA	24750	389312162014-2	12/19/2014	70.91	AWDIRECT	554025 - REPAIR TO EQUIPMEN
STATE BANK OF SOUTHERN UTA	24750	389312162014-2	12/19/2014	364.85	DIESEL DIAGNOSTIC EQUIPMENT	524059 - INTEREST EXPENSE
STATE BANK OF SOUTHERN UTA	24751	5 - 2010 Sewer	12/19/2014	13,635.00	Interest - 2010 Sewer Revenue	522525.2 - 2010 Sewer Revenue r
STATE BANK OF SOUTHERN UTA	24751	5 - 2010 Sewer	12/19/2014	32,000.00	Principal - 2010 Sewer Revenue	102221 - FICA PAYABLE
STATE BANK OF SOUTHERN UTA	12181402	PR121214-424	12/23/2014	735.07	FICA Medicare Tax	

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STATE BANK OF SOUTHERN UTA	12181402	PR121214-424	12/23/2014	735.07	Medicare Tax - Employer	102221 - FICA PAYABLE
STATE BANK OF SOUTHERN UTA	12181402	PR121214-424	12/23/2014	3,143.04	FICA Social Security Tax	102221 - FICA PAYABLE
STATE BANK OF SOUTHERN UTA	12181402	PR121214-424	12/23/2014	3,143.04	Social Security Tax - Employer	102221 - FICA PAYABLE
STATE BANK OF SOUTHERN UTA	12181402	PR121214-424	12/23/2014	4,181.91	FWT	102222 - FEDERAL WITHHOLDIN
STATE BANK OF SOUTHERN UTA	12311402	PR122514-424	12/30/2014	46.93	FWT	102222 - FEDERAL WITHHOLDIN
STATE BANK OF SOUTHERN UTA	12311402	PR122514-424	12/30/2014	62.06	FICA Medicare Tax	102221 - FICA PAYABLE
STATE BANK OF SOUTHERN UTA	12311402	PR122514-424	12/30/2014	62.06	Medicare Tax - Employer	102221 - FICA PAYABLE
STATE BANK OF SOUTHERN UTA	12311402	PR122514-424	12/30/2014	265.38	FICA Social Security Tax	102221 - FICA PAYABLE
STATE BANK OF SOUTHERN UTA	12311402	PR122514-424	12/30/2014	265.38	Social Security Tax - Employer	102221 - FICA PAYABLE
STATE BANK OF SOUTHERN UTA	12311402	PR122614-424	12/30/2014	730.22	FICA Medicare Tax	102221 - FICA PAYABLE
STATE BANK OF SOUTHERN UTA	12311402	PR122614-424	12/30/2014	730.22	Medicare Tax - Employer	102221 - FICA PAYABLE
STATE BANK OF SOUTHERN UTA	12311402	PR122614-424	12/30/2014	3,122.38	FICA Social Security Tax	102221 - FICA PAYABLE
STATE BANK OF SOUTHERN UTA	12311402	PR122614-424	12/30/2014	3,122.38	Social Security Tax - Employer	102221 - FICA PAYABLE
STATE BANK OF SOUTHERN UTA	12311402	PR122614-424	12/30/2014	4,250.37	FWT	102222 - FEDERAL WITHHOLDIN
				\$76,679.73		
STATE OF UTAH GASCARD	24716	NP42966727	12/12/2014	67.31	GAS	105840 - GAS AND OIL
STATE OF UTAH GASCARD	24716	NP42966727	12/12/2014	68.29	GAS	107240 - GAS AND OIL
STATE OF UTAH GASCARD	24716	NP42966727	12/12/2014	98.54	GAS	104340 - Gas & Oil
STATE OF UTAH GASCARD	24716	NP42966728	12/12/2014	542.95	GAS	534040 - GAS AND OIL
STATE OF UTAH GASCARD	24716	NP42966729-1	12/12/2014	97.60	PUBLIC WORKS GAS SPLIT	106140 - GAS AND OIL
STATE OF UTAH GASCARD	24716	NP42966729-1	12/12/2014	97.61	PUBLIC WORKS GAS SPLIT	514040 - GAS AND OIL
STATE OF UTAH GASCARD	24716	NP42966729-1	12/12/2014	97.61	PUBLIC WORKS GAS SPLIT	524040 - GAS AND OIL
STATE OF UTAH GASCARD	24716	NP42966729-1	12/12/2014	97.61	PUBLIC WORKS GAS SPLIT	544040 - GAS AND OIL
STATE OF UTAH GASCARD	24716	NP42966729-1	12/12/2014	97.61	PUBLIC WORKS GAS SPLIT	574040 - GAS AND OIL
STATE OF UTAH GASCARD	24716	NP42966729-2	12/12/2014	8.91	SHOP GAS SPLIT	514040 - GAS AND OIL
STATE OF UTAH GASCARD	24716	NP42966729-2	12/12/2014	8.91	SHOP GAS SPLIT	534040 - GAS AND OIL
STATE OF UTAH GASCARD	24716	NP42966729-2	12/12/2014	8.92	SHOP GAS SPLIT	106140 - GAS AND OIL
STATE OF UTAH GASCARD	24716	NP42966729-2	12/12/2014	8.92	SHOP GAS SPLIT	524040 - GAS AND OIL
STATE OF UTAH GASCARD	24716	NP42966729-2	12/12/2014	8.92	SHOP GAS SPLIT	544040 - GAS AND OIL
STATE OF UTAH GASCARD	24716	NP42966729-2	12/12/2014	8.92	SHOP GAS SPLIT	574040 - GAS AND OIL
STATE OF UTAH GASCARD	24716	NP42966729-2	12/12/2014	142.46	GAS	534040 - GAS AND OIL
STATE OF UTAH GASCARD	24716	NP42966729-2	12/12/2014	159.00	GAS	107040 - GAS AND OIL
STATE OF UTAH GASCARD	24716	NP42966729-2	12/12/2014	254.37	GAS	544040 - GAS AND OIL
STATE OF UTAH GASCARD	24716	NP42966729-2	12/12/2014	326.24	GAS	106140 - GAS AND OIL
STATE OF UTAH GASCARD	24716	NP42966729-2	12/12/2014	1,180.68	GAS	554040 - GAS AND OIL
STATE OF UTAH GASCARD	24716	NP42966730	12/12/2014	36.81	GAS	105740 - GAS AND OIL
STATE OF UTAH GASCARD	24716	NP42966731	12/12/2014	1,424.86	GAS	105440 - GAS AND OIL
				\$4,843.05		
STOCK BUILDING SUPPLY - 1001	24717	51407303-00	12/12/2014	49.05	PUBLIC WORKS SPLIT	106126 - MAINTENANCE, MATER
STOCK BUILDING SUPPLY - 1001	24717	51407303-00	12/12/2014	49.06	PUBLIC WORKS SPLIT	524026 - MAINTENANCE MATER
STOCK BUILDING SUPPLY - 1001	24717	51407303-00	12/12/2014	49.06	PUBLIC WORKS SPLIT	514026 - MAINTENANCE MATER
STOCK BUILDING SUPPLY - 1001	24717	51407303-00	12/12/2014	49.06	PUBLIC WORKS SPLIT	544026 - MAINTENANCE MATER
STOCK BUILDING SUPPLY - 1001	24717	51407303-00	12/12/2014	49.06	PUBLIC WORKS SPLIT	574026 - MAINTENANCE MATER
				\$245.29		
STONES, NATASHA	24718	100000303.1209	12/12/2014	41.94	Deposit Refund: 100000303 - STONES, NATASH	532135 - CUSTOMER DEPOSITS
SUNRISE ENGINEERING, INC	24719	0075325	12/12/2014	43.31	SUNRISE MONTHLY SPLIT	544031 - PROFESSIONAL AND T

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SUNRISE ENGINEERING, INC	24719	0075325	12/12/2014	43.37	SUNRISE MONTHLY SPLIT	524031 - PROFESSIONAL & TEC
SUNRISE ENGINEERING, INC	24719	0075325	12/12/2014	86.67	SUNRISE MONTHLY SPLIT	106131 - PROFESSIONAL AND T
SUNRISE ENGINEERING, INC	24719	0075325	12/12/2014	86.67	SUNRISE MONTHLY SPLIT	514031 - PROFESSIONAL & TEC
SUNRISE ENGINEERING, INC	24719	0075325	12/12/2014	86.67	SUNRISE MONTHLY SPLIT	574031 - PROFESSIONAL AND T
SUNRISE ENGINEERING, INC	24719	0075325	12/12/2014	173.31	SUNRISE MONTHLY SPLIT	534031 - PROFESSIONAL & TEC
				\$520.00		
TELADOC	24720	1250534	12/12/2014	1.24	INSURANCE SPLIT	104214 - INSURANCE
TELADOC	24720	1250534	12/12/2014	1.24	INSURANCE SPLIT	105714 - INSURANCE
TELADOC	24720	1250534	12/12/2014	1.24	INSURANCE SPLIT	105814 - INSURANCE
TELADOC	24720	1250534	12/12/2014	1.24	INSURANCE SPLIT	106914 - INSURANCE
TELADOC	24720	1250534	12/12/2014	1.24	INSURANCE SPLIT	107114 - INSURANCE
TELADOC	24720	1250534	12/12/2014	1.24	INSURANCE SPLIT	108014 - INSURANCE
TELADOC	24720	1250534	12/12/2014	2.48	INSURANCE SPLIT	104114 - INSURANCE
TELADOC	24720	1250534	12/12/2014	6.19	INSURANCE SPLIT	105414 - INSURANCE
TELADOC	24720	1250534	12/12/2014	8.63	INSURANCE SPLIT	104314 - INSURANCE
TELADOC	24720	1250534	12/12/2014	12.38	INSURANCE SPLIT	524014 - INSURANCE
TELADOC	24720	1250534	12/12/2014	12.38	INSURANCE SPLIT	544014 - INSURANCE
TELADOC	24720	1250534	12/12/2014	13.75	INSURANCE SPLIT	574014 - INSURANCE
TELADOC	24720	1250534	12/12/2014	24.75	INSURANCE SPLIT	514014 - INSURANCE
TELADOC	24720	1250534	12/12/2014	24.75	INSURANCE SPLIT	534014 - INSURANCE
TELADOC	24720	1250534	12/12/2014	24.75	INSURANCE SPLIT	554014 - INSURANCE
				\$137.50		
THE GYM ON MAIN	24752	12132014	12/19/2014	434.60	November 2014 membership fees	101590 - GYM MEMBERSHIP
THE KEYMAKER LOCKSMITH SER	24721	17786	12/12/2014	8.00	KEYS FOR PANIC BAR AT FAIRGROUND BUIL	107126 - MAINTENANCE MATERI
THE KEYMAKER LOCKSMITH SER	24753	17797	12/19/2014	31.69	keys and entry lock for electrical tool room	534026 - MAINTENANCE MATERI
				\$39.69		
THE SPECTRUM	24722	0000444916	12/12/2014	200.00	destination magazine article - advertising - Jet Smi	107222 - ADVERTISING
THE SPECTRUM	24754	12152014	12/19/2014	204.02	Spectrum subscription for January through Decem	107521 - BOOKS
				\$404.02		
TIMU, STEVEN	24723	12092014	12/12/2014	997.00	REFUND CASH BAIL FOR BILLY TIMU PER PLE	104237 - BAIL
UTAH EDUCATION NETWORK / U	24724	15-0370	12/12/2014	409.80	GIGE ETHERNET WAN	107531 - PROFESSIONAL & TEC
UTAH RETIREMENT SYSTEMS	12181403	PR121214-487	12/23/2014	17.36	122 - Tier 2 - Police 401K	102230 - RETIREMENT PAYABLE
UTAH RETIREMENT SYSTEMS	12181403	PR121214-487	12/23/2014	24.69	111 - Tier 2 401k	102230 - RETIREMENT PAYABLE
UTAH RETIREMENT SYSTEMS	12181403	PR121214-487	12/23/2014	30.35	122 - Tier 2 - Employer 401K	102230 - RETIREMENT PAYABLE
UTAH RETIREMENT SYSTEMS	12181403	PR121214-487	12/23/2014	70.16	401k Contributions (43) %	102230 - RETIREMENT PAYABLE
UTAH RETIREMENT SYSTEMS	12181403	PR121214-487	12/23/2014	111.56	43 - 401k Loan	102230 - RETIREMENT PAYABLE
UTAH RETIREMENT SYSTEMS	12181403	PR121214-487	12/23/2014	200.00	401k Contributions (43) \$	102230 - RETIREMENT PAYABLE
UTAH RETIREMENT SYSTEMS	12181403	PR121214-487	12/23/2014	207.25	111 - Tier 2 Retirement	102230 - RETIREMENT PAYABLE
UTAH RETIREMENT SYSTEMS	12181403	PR121214-487	12/23/2014	208.33	Roth IRA	102230 - RETIREMENT PAYABLE
UTAH RETIREMENT SYSTEMS	12181403	PR121214-487	12/23/2014	257.30	401k Contributions er (43)	102230 - RETIREMENT PAYABLE
UTAH RETIREMENT SYSTEMS	12181403	PR121214-487	12/23/2014	305.78	122 - Tier 2 - Police Retirement	102230 - RETIREMENT PAYABLE
UTAH RETIREMENT SYSTEMS	12181403	PR121214-487	12/23/2014	350.00	401k Contributions (15) \$	102230 - RETIREMENT PAYABLE
UTAH RETIREMENT SYSTEMS	12181403	PR121214-487	12/23/2014	363.99	457 Employer % (15)	102230 - RETIREMENT PAYABLE
UTAH RETIREMENT SYSTEMS	12181403	PR121214-487	12/23/2014	844.91	457 Plan %	102230 - RETIREMENT PAYABLE

Parowan City
Check Register
General Checking - 12/09/2014 to 01/06/2015

Payee Name	Reference Number	Invoice Number	Payment Date	Amount	Description	Ledger Account
UTAH RETIREMENT SYSTEMS	12181403	PR121214-487	12/23/2014	905.96	401k Contributions (15) %	102230 - RETIREMENT PAYABLE
UTAH RETIREMENT SYSTEMS	12181403	PR121214-487	12/23/2014	936.06	15 - 401k Loan	102230 - RETIREMENT PAYABLE
UTAH RETIREMENT SYSTEMS	12181403	PR121214-487	12/23/2014	1,480.54	401k Contributions er (15)	102230 - RETIREMENT PAYABLE
UTAH RETIREMENT SYSTEMS	12181403	PR121214-487	12/23/2014	1,922.99	43 - Police Retirement	102230 - RETIREMENT PAYABLE
UTAH RETIREMENT SYSTEMS	12181403	PR121214-487	12/23/2014	6,999.86	15 - State Retirement	102230 - RETIREMENT PAYABLE
UTAH RETIREMENT SYSTEMS	12311403	PR122614-487	12/30/2014	17.36	122 - Tier 2 - Police 401K	102230 - RETIREMENT PAYABLE
UTAH RETIREMENT SYSTEMS	12311403	PR122614-487	12/30/2014	24.69	111 - Tier 2 401K	102230 - RETIREMENT PAYABLE
UTAH RETIREMENT SYSTEMS	12311403	PR122614-487	12/30/2014	30.35	122 - Tier 2 - Employer 401K	102230 - RETIREMENT PAYABLE
UTAH RETIREMENT SYSTEMS	12311403	PR122614-487	12/30/2014	70.16	401k Contributions (43) %	102230 - RETIREMENT PAYABLE
UTAH RETIREMENT SYSTEMS	12311403	PR122614-487	12/30/2014	111.56	43 - 401k Loan	102230 - RETIREMENT PAYABLE
UTAH RETIREMENT SYSTEMS	12311403	PR122614-487	12/30/2014	200.00	401k Contributions (43) \$	102230 - RETIREMENT PAYABLE
UTAH RETIREMENT SYSTEMS	12311403	PR122614-487	12/30/2014	207.25	111 - Tier 2 Retirement	102230 - RETIREMENT PAYABLE
UTAH RETIREMENT SYSTEMS	12311403	PR122614-487	12/30/2014	208.33	Roth IRA	102230 - RETIREMENT PAYABLE
UTAH RETIREMENT SYSTEMS	12311403	PR122614-487	12/30/2014	257.30	401k Contributions er (43)	102230 - RETIREMENT PAYABLE
UTAH RETIREMENT SYSTEMS	12311403	PR122614-487	12/30/2014	305.78	122 - Tier 2 - Police Retirement	102230 - RETIREMENT PAYABLE
UTAH RETIREMENT SYSTEMS	12311403	PR122614-487	12/30/2014	334.61	457 Employer % (15)	102230 - RETIREMENT PAYABLE
UTAH RETIREMENT SYSTEMS	12311403	PR122614-487	12/30/2014	350.00	401k Contributions (15) \$	102230 - RETIREMENT PAYABLE
UTAH RETIREMENT SYSTEMS	12311403	PR122614-487	12/30/2014	929.37	457 Plan %	102230 - RETIREMENT PAYABLE
UTAH RETIREMENT SYSTEMS	12311403	PR122614-487	12/30/2014	936.06	15 - 401k Loan	102230 - RETIREMENT PAYABLE
UTAH RETIREMENT SYSTEMS	12311403	PR122614-487	12/30/2014	936.09	401k Contributions (15) %	102230 - RETIREMENT PAYABLE
UTAH RETIREMENT SYSTEMS	12311403	PR122614-487	12/30/2014	1,557.19	401k Contributions er (15)	102230 - RETIREMENT PAYABLE
UTAH RETIREMENT SYSTEMS	12311403	PR122614-487	12/30/2014	1,922.99	43 - Police Retirement	102230 - RETIREMENT PAYABLE
UTAH RETIREMENT SYSTEMS	12311403	PR122614-487	12/30/2014	7,099.95	15 - State Retirement	102230 - RETIREMENT PAYABLE
				\$30,736.13		
UTAH STATE DIVISION OF FINANC	24755	1 - 2013 Parity	12/19/2014	13,000.00	Principal - 2013 Parity Water Revenue	512520.2 - 2013 Parity Water Rev
UTAH STATE DIVISION OF FINANC	24755	1 - 2013 Parity	12/19/2014	21,648.00	Interest - 2013 Parity Water Revenue	514059 - INTEREST EXPENSE
UTAH STATE DIVISION OF FINANC	24755	12 - 2001 Water	12/19/2014	13,181.76	Interest - 2001 Water Revenue	514059 - INTEREST EXPENSE
UTAH STATE DIVISION OF FINANC	24755	12 - 2001 Water	12/19/2014	30,000.00	Principal - 2001 Water Revenue	512545.2 - 2001 Water Revenue r
UTAH STATE DIVISION OF FINANC	24755	4 - 2010 B Sewe	12/19/2014	6,000.00	Principal - 2010 B Sewer Revenue (Collection)	522530.2 - 2010B Sewer Revenue
UTAH STATE DIVISION OF FINANC	24755	4 - 2010 B Sewe	12/19/2014	19,000.00	Principal - 2010 B Sewer Revenue (Treatment)	542510.2 - 2010 DEQ SW LAGOO
UTAH STATE DIVISION OF FINANC	24755	9 - 2005 Sewer	12/19/2014	68,805.00	Interest - 2005 Sewer Revenue	524059 - INTEREST EXPENSE
UTAH STATE DIVISION OF FINANC	24755	9 - 2005 Sewer	12/19/2014	179,000.00	Principal - 2005 Sewer Revenue	522510.2 - 2005 Sewer Lagoons r
				\$350,634.76		
UTAH STATE TAX COMMISSION	24772	L0636242240	12/30/2014	2,445.27	Withholding payment for filing period ending Sep 2	102223 - STATE WITHHOLDING
UTAH STATE TAX COMMISSION	24780	PR112514-490	12/31/2014	31.87	SWT	102223 - STATE WITHHOLDING
UTAH STATE TAX COMMISSION	24780	PR112814-490	12/31/2014	2,125.85	SWT	102223 - STATE WITHHOLDING
UTAH STATE TAX COMMISSION	24780	PR121214-490	12/31/2014	2,107.44	SWT	102223 - STATE WITHHOLDING
UTAH STATE TAX COMMISSION	24780	PR122514-490	12/31/2014	31.87	SWT	102223 - STATE WITHHOLDING
UTAH STATE TAX COMMISSION	24780	PR122614-490	12/31/2014	2,143.00	SWT	102223 - STATE WITHHOLDING
				\$8,885.30		
WARNER TRUCK CENTER	24725	513484	12/12/2014	416.03	PART FOR '04 CURB TENDER GARBAGE TRU	554025 - REPAIR TO EQUIPMEN
WHEELER MACHINERY CO	24726	PS000145687	12/12/2014	168.79	OIL SAMPLE KITS	554025 - REPAIR TO EQUIPMEN
WHEELER MACHINERY CO	24726	PS000149870	12/12/2014	51.60	HYDRAULIC HOSE - JD 1600 MOWER	107025 - REPAIRS TO EQUIPME
WHEELER MACHINERY CO	24726	PS000158375	12/12/2014	128.06	COUPLINGS, SEALS, HOSES, LABOR, MISC. C	106125 - REPAIR TO EQUIPMEN
WHEELER MACHINERY CO	24771	PS000165489	12/23/2014	27.18	SHOP SPLIT	106126 - MAINTENANCE, MATER
WHEELER MACHINERY CO	24771	PS000165489	12/23/2014	27.18	SHOP SPLIT	524026 - MAINTENANCE MATER

Parowan City
Check Register
General Checking - 12/09/2014 to 01/06/2015

Payee Name	Reference Number	Invoice Number	Payment Date	Amount	Description	Ledger Account
WHEELER MACHINERY CO	24771	PS000165489	12/23/2014	27.18	SHOP SPLIT	544026 - MAINTENANCE MATERI
WHEELER MACHINERY CO	24771	PS000165489	12/23/2014	27.18	SHOP SPLIT	574026 - MAINTENANCE MATERI
WHEELER MACHINERY CO	24771	PS000165489	12/23/2014	27.19	SHOP SPLIT	514026 - MAINTENANCE MATERI
WHEELER MACHINERY CO	24771	PS000165489	12/23/2014	27.25	SHOP SPLIT	534026 - MAINTENANCE MATERI
				<u>\$511.61</u>		
WILLIAM L PRATER, LLC	24756	12172014	12/19/2014	5,680.00	LEGAL SERVICES FOR CENTER CREEK HYDR	531601 - Electric work in process
YARDLEY, MARK	24727	100000138.1205	12/12/2014	117.61	Deposit Refund: 100000138 - YARDLEY, MARK	532135 - CUSTOMER DEPOSITS
				<u>\$571,402.06</u>		

Dated _____

Mayor _____

City Council _____

City Recorder

City Treasurer

NOT

**LOCAL BUILDING AUTHORITY OF
PAROWAN CITY, UTAH
PARAMETERS RESOLUTION
January 8, 2015**

RESOLUTION NO. LBA 2015-01-01

A RESOLUTION OF THE LOCAL BUILDING AUTHORITY OF PAROWAN CITY, UTAH, CALLING A PUBLIC HEARING AND AUTHORIZING NOTICE OF INTENTION TO ISSUE NOT MORE THAN \$1,100,000 OF LEASE REVENUE BONDS FOR THE CONSTRUCTION OF A CITY ADMINISTRATIVE BUILDING AND PUBLIC SAFETY FACILITY, AND RELATED IMPROVEMENTS; FIXING THE MAXIMUM INTEREST RATE, MATURITY AND DISCOUNT OF THE BONDS; AND RELATED MATTERS.

WHEREAS, by resolution adopted by the City Council of Parowan City (the "City Council" and "City," respectively), the City authorized, approved and directed, the creation of the Local Building Authority of Parowan City, Utah (the "Authority"), under the Utah Local Building Authority Act, Title 17D, Chapter 2, Utah Code Annotated 1953, as amended (the "Act"), for the purpose of accomplishing the public purposes for which the City exists by acquiring, improving or extending one or more projects, as defined in the Act, and financing their costs on behalf of the City; and

WHEREAS, pursuant to the provisions of the Act, the Authority Board of the Authority has authority to issue lease revenue bonds in one or more series for the purpose of financing certain local improvements for and on behalf of the City; and

WHEREAS, the Authority and the City desire to acquire and construct a city administrative building and a public safety facility and related improvements (the "Project") and finance the Project with the proceeds of bonds (the "Bonds") of one or more series in the maximum principal amount of \$1,100,000 and in the form of Lease Revenue Bonds to bear interest at a rate or rates not to exceed 1.5% per annum, to be issued by the Authority; and

WHEREAS, the Act requires the Authority to call a public hearing and provides for the publication of a Notice of Public Hearing and Bonds to be Issued, and the Authority desires to call such hearing and publish such a notice at this time in compliance with the Act with respect to the Bonds.

NOW, THEREFORE, it is hereby resolved by the Authority Board of the Local Building Authority of Parowan City, Utah as follows:

Section 1. The Authority Board of the Authority hereby finds and determines that it is in the best interest of the Authority, the City and its residents for the Authority to issue not more than \$1,100,000 aggregate principal amount of its Lease Revenue Bonds in one or more series (the "Bonds"), to bear interest at a rate or rates not

to exceed 1.5% per annum, to mature in not more than 35 years from their date or dates, and to be sold at a discount of not less than 99% of the total principal amount thereof for the purpose of financing in part the acquisition and construction a city administrative building and a public safety facility and related improvements (the "Project"), all pursuant to this Resolution, and an Authorizing Resolution and Master Resolution to be adopted and approved by the Authority Board authorizing and confirming the issuance and sale of the Bonds in substantially the forms attached hereto as Exhibit A (referred to herein collectively as the "Final Bond Resolutions"), and the Authority hereby declares its intention to issue the Bonds according to the provisions of this Resolution and the Final Bond Resolutions, when adopted, and to sell the Bonds.

Section 2. The Authority hereby authorizes and approves the issuance and sale of the Bonds, pursuant to the provisions of this Resolution and the Final Bond Resolutions to be adopted by the Authority Board authorizing and confirming the issuance and sale of the Bonds, with such changes thereto as shall be approved by the Authority Board upon the adoption of the Final Bond Resolutions, provided the terms of the Bonds fall within the parameters set forth herein in Section 1.

Section 3. The Authority calls a public hearing for February 5, 2015, at 6:00 p.m. or as soon thereafter as feasible, to receive public input on the issuance of the Bonds and on the economic impact of the proposed Project on the private sector.

Section 4. In accordance with the provisions of the Act, the Secretary of the Authority shall cause a "Notice of Public Hearing and Bonds to be Issued," substantially in the form attached hereto as Exhibit B, to be (1) published once each week for two consecutive weeks in a newspaper of general circulation in the Authority and (2) posted on the Utah Public Notice Website, at least 14 days before the Public Hearing, and shall cause a copy of this Resolution (together with all exhibits hereto) to be kept on file in the office of the Authority's Secretary in Parowan, Utah, for public examination during the regular business hours of the Secretary for at least 30 days from and after the last date of publication of the above referenced notice.

Section 5. The Authority hereby appoints the Mayor of the City as the Chair of the Authority and the City Recorder as the Secretary of the Authority. And, the Authority adopts and ratifies the bylaws

Section 6. This declaration is intended to be a declaration of official intent under Treasury Regulation § 1.103-18(1).

Section 7. The Secretary of the Authority is directed to complete the attached Record of Proceedings to officially record the proceedings at which this Resolution was considered for adoption.

Section 8. All resolutions or parts thereof in conflict herewith are, to the extent of such conflict, hereby repealed and this Resolution shall be in full force and effect immediately upon its approval and adoption.

APPROVED AND ADOPTED this January 8, 2015.



Chair

ATTEST AND COUNTERSIGN:


Secretary

RECORD OF PROCEEDINGS

The Authority Board of the Local Building Authority of Parowan City, Utah (the "Authority") met in public session at its regular meeting place, at 16 South Main Street, Parowan, Utah 84761, on January 8, 2015 (the "Meeting"), at the hour of 6:00 p.m., or as soon thereafter as feasible, with the following members of the Board being present:

Donald Landes	Chair
Alan Adams	Boardmember
Troy Houston	Boardmember
Ben Johnson	Boardmember
Steve Thayer	Boardmember
Steve Watson Weston	Boardmember

Also present:

Callie Bassett	Secretary
Shayne Scott	City Manager

Absent:

which constituted all the members thereof.

After the Meeting had been duly called to order and after other matters were discussed, the foregoing resolution (the "Resolution") was introduced in written form and fully discussed.

A motion to adopt the Resolution was then duly made by Steve Thayer and seconded by Steve Weston, and the Resolution was put to a vote and carried, the vote being as follows:

Those voting YEA: 5

Those voting NAY: 0

Those Abstaining: 0

Other business not pertinent to the Resolution appears in the minutes of the Meeting. Upon the conclusion of all business on the Agenda and motion duly made and carried, the Meeting was adjourned.

CERTIFICATE OF AUTHORITY SECRETARY

I, Callie Bassett, the duly appointed and qualified Secretary of the Local Building Authority of Parowan City, Utah (the "Authority"), do hereby certify that the attached Resolution is a true, accurate and complete copy thereof as adopted by the Authority Board of the Authority at a public meeting duly held on January 8, 2015 (the "Meeting"). The Meeting was called and noticed as required by law as is evidenced by the attached Meeting Notice and Certificate of Compliance with Open Meeting Law. The persons present and the result of the vote taken at the Meeting are all as shown above. The Resolution, with all exhibits attached, was deposited in my office on January 8, 2015, and is officially of record in my possession.

IN WITNESS WHEREOF, I have hereunto subscribed my signature and impressed hereon the official seal of the Authority, this January 8, 2015.

(L B A S E A)



Callie Bassett
Secretary

CERTIFICATE OF COMPLIANCE WITH
OPEN MEETING LAW

I, Callie Bassett, the undersigned Secretary of the Local Building Authority of Parowan City, Utah (the "Authority") do hereby certify, according to the records of the Authority in my official possession, and upon my own knowledge and belief, that in accordance with the requirements of Section 52-4-202, Utah Code Annotated, 1953, as amended, I gave not less than twenty-four (24) hours public notice of the agenda, date, time and place of the January 8, 2015, public meeting held by the Authority (the "Meeting") as follows:

(a) By causing a "Meeting Notice," in the form attached, to be posted at the principal office of the Authority at least 24 hours prior to the convening of the Meeting, the Meeting Notice having continuously remained so posted and available for public inspection until the completion of the meeting;

(b) By causing a copy of the Meeting Notice to be delivered to a newspaper of general circulation in the geographic jurisdiction of the Authority at least 24 hours prior to the convening of the Meeting;

(c) By causing a copy of the Meeting Notice to be posted on the Utah Public Notice Website at least 24 hours prior to the convening of the Meeting; and

(d) By causing notice of the Meeting to be personally provided to each and every member of the Authority Board of the Authority at least 24 hours prior to the convening of the Meeting.

IN WITNESS WHEREOF, I have hereunto subscribed my official signature this January 8, 2015.




Secretary

(Attach the Meeting Notice and proof of posting thereof on the Utah Public Notice Website)

Callie Bassett

From: support@utahinteractive.org
Sent: Wednesday, January 07, 2015 8:03 AM
To: pmn-1290@listserv.utah.gov
Subject: Public Notice for Parowan City Council

Utah Public Notice

Parowan City Council

Parowan City Council Meeting Agenda

Notice Date & Time: 1/8/15 6:00 PM

Description/Agenda:

AGENDA

PAROWAN CITY COUNCIL MEETING

January 8, 2015

Library Lounge, 16 South Main, 6:00 P.M.

1. Call Meeting to Order
2. Opening Ceremonies/Thought/Prayer ? Mayor Don Landes
Pledge of Allegiance - Troop 341
3. Does anybody have any conflicts or personal interest in any matter on the agenda which needs to be declared?

CONSENT MEETING

4. Approval of Minutes (December 11, 2014 City Council Meeting)
5. Purchase Orders/Warrant Register
6. Airport Board Appointment, Jay Orton (to replace Gordon Birch)

LOCAL BUILDING AUTHORITY BUSINESS

7. A RESOLUTION OF THE LOCAL BUILDING AUTHORITY OF PAROWAN CITY, UTAH, CALLING A PUBLIC HEARING AND AUTHORIZING NOTICE OF INTENTION TO ISSUE NOT MORE THAN \$1,100,000 OF LEASE REVENUE BONDS FOR THE CONSTRUCTION OF A CITY ADMINISTRATIVE BUILDING AND A PUBLIC SAFETY FACILITY, AND RELATED IMPROVEMENTS; FIXING THE MAXIMUM INTEREST RATE, MATURITY AND DISCOUNT OF THE BONDS; AND RELATED MATTERS.
RESOLUTION 2015-01-01

ACTION MEETING

8. A RESOLUTION OF PAROWAN CITY RATIFYING THE CREATION OF THE LOCAL BUILDING AUTHORITY OF PAROWAN CITY, UTAH (THE "AUTHORITY") AND APPROVING THE ADOPTION BY THE AUTHORITY OF A PARAMETERS RESOLUTION AUTHORIZING NOT MORE THAN \$1,100,000 LEASE REVENUE BONDS FOR THE CONSTRUCTION OF A CITY ADMINISTRATIVE BUILDING AND A PUBLIC SAFETY FACILITY, AND RELATED MATTERS. RESOLUTION 2015-01-02
9. Transportation Agreement with ULCT/Salt Lake City Chamber of Commerce
10. Center Creek Hydro Amendment ? Sunrise Engineering

WORK MEETING

3/3/2015

11. Road Maintenance Plan Presentation ? LTAP
12. Impact Fee Credits Application ? Mr. Ken Allen
13. Shade Tree Presentation ? Vern Fridley
14. Sewer System Management Plan ? Aldo Biasi
15. Parowan City Goals for 2015
16. Trails Master Plan RFQ ? TARP recommendation
17. Open Meeting Laws Training ? Justin Wayment
18. HR Manual Change regarding Workers Compensation Medical Facility
19. Member Reports
20. Public comment & discussion - Two minute limit each
21. Adjourn

CERTIFICATE OF POSTING & FAXING

I hereby certify that on the 6th day of January, 2015 I posted a copy of the foregoing agenda at the Parowan City Office, Parowan City Library, on the State web site, on the City web site, and I faxed a copy to The Spectrum at 586-7471

Callie Bassett, City Recorder

Notice of Special Accommodations:

Notice: Persons with disabilities needing assistance to participate in this meeting should contact the City Recorder at (435) 477-3331 no later than 24 hours prior to the meeting.

Notice of Electronic or telephone participation:

NA

Other information:

Location:

16 S Main, Parowan, 84761

Contact information:

Valorie Topham, vtopham@netutah.com, 435-477-3331

EXHIBIT A

FINAL BOND RESOLUTIONS

(See Transcript Documents Nos. __ and __)

EXHIBIT B

NOTICE OF PUBLIC HEARING AND BONDS TO BE ISSUED

PUBLIC NOTICE IS HEREBY GIVEN THAT the governing board of the Local Building Authority of Parowan City, Utah (the "Authority Board" and the "Authority," respectively) adopted a resolution (the "Resolution") declaring its intention to issue lease revenue bonds (the "Bonds") pursuant to the Utah Local Building Authority Act, Title 17D, Chapter 2, Utah Code Annotated 1953, as amended, and calling the public hearing described below.

TIME, PLACE AND LOCATION OF PUBLIC HEARING

The Authority shall hold a public hearing on February 5, 2015, at 6:00 p.m. or as soon thereafter as possible. The location of the public hearing is at the Authority's offices at 16 South Main Street, Parowan, Utah 84761. The purpose of the public hearing is to receive input from the public with respect to the issuance of the Bonds and the potential economic impact that the improvement, facility, or property for which the Bonds will pay all or part of the cost will have on the private sector. All members of the public are invited to attend and participate.

PARAMETERS OF THE BONDS

The Authority intends to issue the Bonds as Lease Revenue Bonds in an aggregate principal amount of not to exceed \$1,100,000, to bear interest at the rate or rates of not to exceed 1.5% per annum, to mature in not more than thirty-five (35) years from their date or dates, and to be sold at a price not less than ninety-nine percent (99%) of the total principal amount thereof, plus accrued interest to the date of delivery. No taxes will be pledged for the repayment of the Bonds. The security of the Bonds will be the facilities constructed. The Bonds will not be on parity with any other Bonds of the Authority or the City or secured by the same revenues as any other bonds. If Bonds are issued in the full amount above, and carried to maximum maturity, at the maximum interest rate, then the amount to be repaid will be approximately \$1,414,572. However, the City and Authority have obtained a funding commitment from the State of Utah Permanent Community Impact Fund Board for a grant in the amount of \$1,458,000 and a loan in the amount of \$972,000 and anticipate that the Bonds will not be issued in an amount in excess of \$972,000 at an interest rate of not more than 1.5% per annum, to be repaid over 30 years, in which event the amount to be repaid will be approximately \$1,207,644. The City and Authority will not have any obligation to repay the grant of \$1,458,000. There are no other bonds currently outstanding that are secured by the same pledge of revenues as the proposed Bonds.

PURPOSE FOR ISSUING BONDS

The Bonds will be issued pursuant to the Resolution, and an Authorizing Resolution and Master Resolution (collectively, the "Final Bond Resolutions") to be adopted by the Authority Board authorizing and confirming the sale of the Bonds for the

purpose of (i) financing in part the acquisition and construction of a city administrative building and a public safety facility and related improvements for the benefit of Parowan City, Utah (the "City") (the "Project"); (ii) paying expenses to be incurred in connection with the issuance and sale of the Bonds.

A draft of the Final Bond Resolutions in substantially final form was before the Authority Board and was part of the Resolution at the time of the adoption of the Resolution by the Authority Board. The Final Bond Resolutions will be adopted by the Authority Board in such form and with such changes thereto as shall be approved by the Authority Board upon the adoption thereof; provided that the principal amount, interest rate, maturity and discount of the Bonds will not exceed the maximums set forth above.

Copies of the Final Bond Resolutions and information on the Authority's outstanding bonds are on file in the office of the Secretary of the Authority at the City offices where they may be examined during regular business hours of the Secretary from 8:30 a.m. to 5:00 p.m., Monday through Friday, for a period of at least thirty (30) days from and after the last date of publication of this notice.

NOTICE IS FURTHER GIVEN that, for a period of thirty (30) days from and after the last date of the publication of this notice (the "30-day Period"), (i) any person in interest shall have the right to contest the legality of the Final Bond Resolutions, the Bonds, or any provision made for the security and payment of the Bonds by filing a verified written complaint in the district court of the county in which he or she resides, and that after the 30-day period, no one shall have any cause of action to contest the regularity, formality or legality thereof for any cause for any reason, and (ii) registered voters within the City may sign a written petition requesting an election to authorize the issuance of the Bonds. If written petitions which have been signed by at least 20% of the registered voters within the City are filed with the Authority during the 30-day Period, the Authority shall be required to hold an election to obtain voter authorization prior to the issuance of the Bonds. If fewer than 20% of the registered voters within the City file a written petition during the 30-day Period, the Authority may proceed to issue the Bonds without an election.

/s/ Callie Bassett
Secretary
Local Building Authority of
Parowan City, Utah

4843-9304-7073, v. 1

4843-9304-7073/PA003-007

**CITY COUNCIL OF
PAROWAN CITY, UTAH
PARAMETERS RESOLUTION
January 8, 2015**

RESOLUTION NO. 2015-01-01

A RESOLUTION OF PAROWAN CITY RATIFYING THE CREATION OF THE LOCAL BUILDING AUTHORITY OF PAROWAN CITY, UTAH (THE "AUTHORITY") AND APPROVING THE ADOPTION BY THE AUTHORITY OF A PARAMETERS RESOLUTION AUTHORIZING NOT MORE THAN \$1,100,000 LEASE REVENUE BONDS FOR THE CONSTRUCTION OF A CITY ADMINISTRATIVE BUILDING AND A PUBLIC SAFETY FACILITY, AND RELATED MATTERS.

WHEREAS, on August 28, 2014, Parowan City, Iron County, Utah (the "City") adopted a resolution (the "Creating Resolution") authorizing the creation of the Local Building Authority of Parowan City, Utah (the "Authority") pursuant to provisions of the Utah Local Building Authority Act, Title 17D, Chapter 2, Utah Code Annotated 1953, as amended, (the "Act"); and

WHEREAS, the City intends to authorize the Authority to finance the acquisition and construction of an administrative building and a public safety facility, and related improvements for the benefit of Parowan City (the "Project"); and

WHEREAS, the City now desires to confirm and ratify the Creating Resolution and adoption of bylaws and all action taken with respect to the creation of the Authority and authorize the Authority to adopt a resolution to call a public hearing and provide notice thereof and establish the parameters for issuance of its Lease Revenue Bonds for the financing of the Project.

NOW, THEREFORE, it is hereby resolved by the City Council of the City as follows:

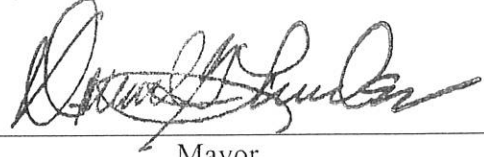
Section 1. The Council hereby ratifies the Creating Resolution and all action taken with respect to the creation of the Authority on and after August 28, 2014, including the bylaws thereof.

Section 2. The Authority is hereby authorized to adopt a resolution setting the parameters for the issuance of its Lease Revenue Bonds pursuant to a parameters resolution, the form of which is attached hereto as Exhibit A and made a part hereof by reference.

Section 3. All resolutions or parts thereof in conflict herewith are, to the extent of such conflict, hereby repealed and this Resolution shall be in full force and effect immediately upon its approval and adoption.

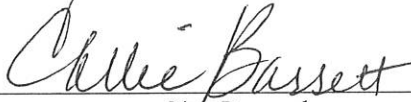
Section 4. The City Recorder is directed to complete the attached Record of Proceedings to officially record the proceedings at which this Parameters Resolution was considered for adoption.

APPROVED AND ADOPTED this January 8, 2015.



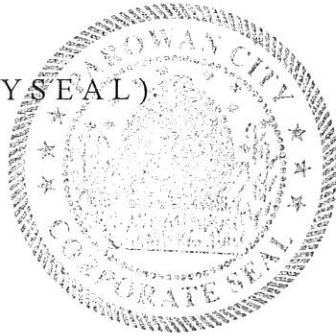
Mayor

ATTEST:



City Recorder

(CITY SEAL)



RECORD OF PROCEEDINGS

I, Callie Bassett, the City Recorder for Parowan City (the "City"), certify that the City Council of the City (the "Council") met in public session at the regular meeting place of the Council, at 16 South Main Street, Parowan, Utah 84761, on January 8, 2015 (the "Meeting") at 6:00 p.m. There were present at that meeting the following members:

Donald Landes	Mayor
Alan Adams	Councilmember
Troy Houston	Councilmember
Ben Johnson	Councilmember
Steve Thayer	Councilmember
Steve Watson <u>Weston</u>	Councilmember

Also present:

Callie Bassett	City Recorder
Shayne Scott	City Manager

Absent:

which constituted all the members thereof.

After the Meeting had been duly called to order and after other matters were discussed, the foregoing resolution (the "Resolution") was introduced in written form and fully discussed.

A motion to adopt the Resolution was then duly made by Alan Adams and seconded by Ben Johnson, and the Resolution was put to a vote and carried, the vote being as follows:

Those voting YEA: 5

Those voting NAY: 0

Those Abstaining: 0

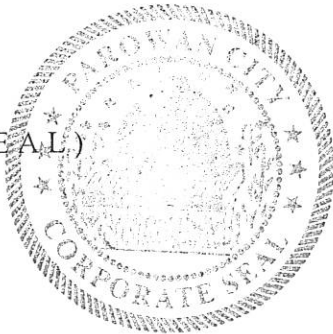
Other business not pertinent to the Resolution appears in the minutes of the Meeting. Upon the conclusion of all business on the Agenda and motion duly made and carried, the Meeting was adjourned.

CERTIFICATE OF CITY RECORDER

I, Callie Bassett, the duly appointed and qualified City Recorder for Parowan City, Utah (the "City"), do hereby certify that the attached Resolution is a true, accurate and complete copy thereof as adopted by the City Council of the City at a public meeting duly held on January 8, 2015 (the "Meeting"). The Meeting was called and noticed as required by law as is evidenced by the attached Meeting Notice and Certificate of Compliance with Open Meeting Law. The persons present and the result of the vote taken at the Meeting are all as shown above. The Resolution, with all exhibits attached, was deposited in my office on January 8, 2015, and is officially of record in my possession.

IN WITNESS WHEREOF, I have hereunto subscribed my signature and impressed hereon the official seal of the City, this January 8, 2015.

(CITY SEAL)





City Recorder

CERTIFICATE OF COMPLIANCE WITH OPEN MEETING LAW

I, Callie Bassett, the City Recorder for Parowan City, Utah (the "City"), certify according to the records of the City in my official possession, and upon my own knowledge and belief, that in accordance with the requirements of Section 52-4-202, Utah Code Annotated 1953, as amended, I gave not less than twenty-four (24) hours public notice of the agenda, date, time, and place of the January 8, 2015, public meeting held by the City Council of the City (the "Meeting") as follows:

(a) By causing a Meeting Notice, in the form attached, to be posted at the City's principal offices at least 24 hours prior to the convening of the Meeting, the Meeting Notice having continuously remained so posted and available for public inspection until the completion of the Meeting;

(b) By causing a copy of the Meeting Notice to be delivered to a newspaper of general circulation in the City at least 24 hours prior to the convening of the Meeting; and

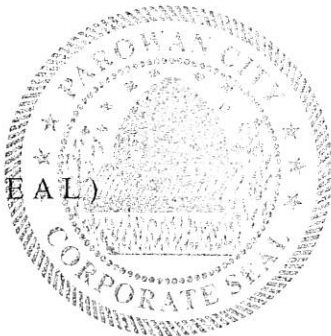
(c) By causing a copy of the Meeting Notice to be posted on the Utah Public Notice Website at least 24 hours prior to the convening of the Meeting.

In addition, the Notice of 2015 Annual Meeting Schedule for the Issuer, attached hereto, specifying the date, time and place of the regular meetings of the governing body of the Issuer to be held during the calendar year 2015 was posted on January 6, 2015, at the principal offices of the Issuer and provided to at least one newspaper of general circulation within the geographic jurisdiction of the Issuer on January 6, 2015, and posted on the Utah Public Notice Website on January 6, 2015.

DATED: January 8, 2015.

Callie Bassett
City Recorder

(CITY SEAL)



(Attach Meeting Notice, including proof of posting thereof on the Utah Public Notice Website)

Callie Bassett

From: support@utahinteractive.org
Sent: Wednesday, January 07, 2015 8:03 AM
To: pmn-1290@listserv.utah.gov
Subject: Public Notice for Parowan City Council

Utah Public Notice

Parowan City Council

Parowan City Council Meeting Agenda

Notice Date & Time: 1/8/15 6:00 PM

Description/Agenda:

AGENDA

PAROWAN CITY COUNCIL MEETING

January 8, 2015

Library Lounge, 16 South Main, 6:00 P.M.

1. Call Meeting to Order
2. Opening Ceremonies/Thought/Prayer ? Mayor Don Landes
Pledge of Allegiance - Troop 341
3. Does anybody have any conflicts or personal interest in any matter on the agenda which needs to be declared?

CONSENT MEETING

4. Approval of Minutes (December 11, 2014 City Council Meeting)
5. Purchase Orders/Warrant Register
6. Airport Board Appointment, Jay Orton (to replace Gordon Birch)

LOCAL BUILDING AUTHORITY BUSINESS

7. A RESOLUTION OF THE LOCAL BUILDING AUTHORITY OF PAROWAN CITY, UTAH, CALLING A PUBLIC HEARING AND AUTHORIZING NOTICE OF INTENTION TO ISSUE NOT MORE THAN \$1,100,000 OF LEASE REVENUE BONDS FOR THE CONSTRUCTION OF A CITY ADMINISTRATIVE BUILDING AND A PUBLIC SAFETY FACILITY, AND RELATED IMPROVEMENTS; FIXING THE MAXIMUM INTEREST RATE, MATURITY AND DISCOUNT OF THE BONDS; AND RELATED MATTERS.
RESOLUTION 2015-01-01

ACTION MEETING

8. A RESOLUTION OF PAROWAN CITY RATIFYING THE CREATION OF THE LOCAL BUILDING AUTHORITY OF PAROWAN CITY, UTAH (THE "AUTHORITY") AND APPROVING THE ADOPTION BY THE AUTHORITY OF A PARAMETERS RESOLUTION AUTHORIZING NOT MORE THAN \$1,100,000 LEASE REVENUE BONDS FOR THE CONSTRUCTION OF A CITY ADMINISTRATIVE BUILDING AND A PUBLIC SAFETY FACILITY, AND RELATED MATTERS. RESOLUTION 2015-01-02
9. Transportation Agreement with ULCT/Salt Lake City Chamber of Commerce
10. Center Creek Hydro Amendment ? Sunrise Engineering

WORK MEETING

11. Road Maintenance Plan Presentation ? LTAP
12. Impact Fee Credits Application ? Mr. Ken Allen
13. Shade Tree Presentation ? Vern Fridley
14. Sewer System Management Plan ? Aldo Biasi
15. Parowan City Goals for 2015
16. Trails Master Plan RFQ ? TARP recommendation
17. Open Meeting Laws Training ? Justin Wayment
18. HR Manual Change regarding Workers Compensation Medical Facility
19. Member Reports
20. Public comment & discussion - Two minute limit each
21. Adjourn

CERTIFICATE OF POSTING & FAXING

I hereby certify that on the 6th day of January, 2015 I posted a copy of the foregoing agenda at the Parowan City Office, Parowan City Library, on the State web site, on the City web site, and I faxed a copy to The Spectrum at 586-7471

Callie Bassett, City Recorder

Notice of Special Accommodations:

Notice: Persons with disabilities needing assistance to participate in this meeting should contact the City Recorder at (435) 477-3331 no later than 24 hours prior to the meeting.

Notice of Electronic or telephone participation:

NA

Other information:

Location:

16 S Main, Parowan, 84761

Contact information:

Valorie Topham, vtopham@netutah.com, 435-477-3331

Public Notice Website

- [About](#)
- [Login](#)
- [Help](#)

Welcome to the Utah Public Notice Website: Your central source for all public notice information in Utah

[Search again](#)

Parowan: Parowan City Council

Entity: Parowan

Public Body: [Parowan City Council](#)

Subject: Public Meetings

Notice Title: Public Notice of Annual Meetings for 2015

Notice Type: Notice

Notice Date & Time: January 6, 2015 | 9:17 AM - 9:17 AM

Description/Agenda:

PUBLIC NOTICE OF ANNUAL MEETINGS FOR 2014

Pursuant to Section 52-4-6, Utah Code annotated 1953, Parowan City hereby gives notice that Parowan City Council holds its regular meetings beginning in January on the following dates in the year 2015.

January 08, 2014

July 9, 2014

January 22, 2014

July 23, 2014

February 12, 2014

August 13, 2014

February 26, 2014

August 27, 2014

March 12, 2014

No Meeting - ULC&T Annual
Convention

March 26, 2014

September 24, 2014

April 9, 2014

October 08, 2014

April 23, 2014

October 22, 2014

May 14, 2014

November 12, 2014

May 28, 2014

No Meeting - Thanksgiving

June 11, 2014

December 10, 2014

June 25, 2014

No Meeting - Christmas

Meetings will be held at the Parowan City Library Lounge, 16 South Main, Parowan, Utah, starting at 6:00 P.M.

Persons or groups wishing to address the City Council must complete and submit an application no less than 7 days prior to the City Council Meeting date by 11:00 a.m. to the Parowan City Recorder.

Donald G. Landes, Mayor

Callie Bassett, City Recorder

Posted January 6, 2015

Notice of Special Accommodations:

Notice: Persons with disabilities needing assistance to participate in this meeting should contact the City Recorder at (435) 477-3331 no later than 24 hours prior to the meeting.

Notice of Electronic or telephone participation:

NA

Other information:

This notice was posted on: January 06, 2015 09:25 AM

This notice was last edited on: January 06, 2015 09:25 AM

[Please give us feedback](#)

Meeting Location:

16 S Main
Parowan , 84761

[Map this!](#)

Contact Information:

Valorie Topham
435-477-3331
vtopham@netutah.com

Audio File Address

Subscription options

Subscription options will send you alerts regarding future notices posted by this public body.

- [RSS](#)
- [E-mail](#)

Options

- [Add this notice to calendar](#)
- [Printer Friendly](#)
- [Email this to a Friend](#)

Connect!

Tweet 0

Like Be the first of your friends to like this.

EXHIBIT A

AUTHORITY PARAMETERS RESOLUTION

(See Transcript Document No. __)

4838-7288-7841, v. 1



October 27, 2014

Mayor Donald Landes, Parowan City
PO Box 576
Parowan, UT 84761

Dear Mayor Landes,

Whether you drive on roads, bike on paths, cruise on ATVs, hop on the bus, or walk on the sidewalk, transportation is a part of your daily life. Which roads do you avoid? Where does your sidewalk end? How often do your kids stay inside because of the inversion? You hear from residents how they expect not only well-maintained roads but also transit, ATV, and active transportation options. You have to do more with less and the traditional resources are diminishing. We live in a new era of transportation—we must have a new vision for funding it.

At this year's Utah League of Cities and Towns Annual Convention, the ULCT membership passed a resolution that identified the need for transportation funding and recommended a legislative solution. **We must expand funding for local transportation NOW.**

We recognize the power in numbers. The Utah League of Cities and Towns, Utah Association of Counties, and the Salt Lake Chamber have formed the Utah Transportation Coalition. The Coalition's goal is to build support for major investment in Utah's transportation system per Utah's Unified Transportation Plan, preserve Utah's quality of life, bolster economic growth, improve personal health and air quality, and provide maximum value to all Utahns.

The Coalition will roll out a communications campaign to generate public and political support for comprehensive transportation solutions and to fund the Unified Transportation Plan across the state. We have provided a sample service agreement for your city/town to review, prepare, and enact to join the Coalition. The Coalition will provide a communication toolkit that you can use as is (without additional staff work) or personalize the materials for your community, including newsletter messages, utility fee inserts, social media messages and a city council resolution.

We need Parowan's financial support of \$100 to join together with all other Utah cities, towns, counties, and chambers. The private sector has pledged the majority of the needed amount and they are asking for local government to stand shoulder to shoulder in the effort—a public-private partnership that will make a difference. Please adopt a service agreement (based on the enclosed sample) and support the Coalition. For more information, contact Abby Albrecht at the Utah Transportation Coalition at (801) 831-6116 or at abby.albrecht@gcinc.com.

Thank you for your partnership and your support.

Lane Beattie
President/CEO of Salt Lake Chamber

Ken Bullock
Executive Director, Utah League of Cities & Towns



Project Name: Utah Transportation Coalition / Salt Lake Chamber

AN AGREEMENT FOR PROFESSIONAL SERVICES BETWEEN

CITY
and
Salt Lake Chamber of Commerce

THIS AGREEMENT made and entered into this ____ day of November, 2014, by and between _____, a municipal corporation (hereinafter referred to as "**City**"), and SALT LAKE CHAMBER (hereinafter referred to as "**Consultant**").

The **City** and **Consultant** agree as follows:

1. **RETENTION AS CONSULTANT**

City hereby retains **Consultant**, and **Consultant** hereby accepts such engagement, to perform the services described in Paragraph 2. **Consultant** warrants it has the qualifications, experience and facilities to properly perform said services.

2. **DESCRIPTION OF SERVICES**

Task 1: Transportation Issues Research and Analysis:

The **Consultant** shall research and analyze transportation funding in Utah at both the State and local level, and use this data to suggest improvements and enhancements to funding transportation in Utah.

These Services shall be completed on June 30, 2015.

Task 2: Transportation Issue Advocacy and Public Awareness Campaign:

The **Consultant** shall create an issue advocacy and public awareness campaign related to Utah's need for improved transportation, and how improved transportation can benefit Utah's economy, air quality, and quality of life. This advocacy and public awareness campaign will include strategic communications planning, advertising media, advertising purchases, public events, online media, social media, editorial content, and other communications tools.

These Services shall be completed on June 30, 2105.

Task 3: Transportation Issue Local Government Tool Kit:

The **Consultant** shall deliver to each municipality a Transportation advocacy tool kit, consisting of but not limited to social media content, utility bill insert content, a city specific fact sheet detailing transportation funding in the individual municipality, editorial content for local papers, website content, and other items to support and aid local governments in discussing their transportation needs with residents.

These Services shall be completed on June 30, 2015.

Task 4: Legislative and Governmental Relations:

The **Consultant** shall work with the Utah League of Cities and Towns and the Utah Association of Counties to educate legislators about state and local transportation funding issues. No lobbyists will be engaged in this effort; however individuals required by State law to register as lobbyists working on behalf of these organizations will be involved.

These Services shall be completed on June 30, 2015.

3. **COMPENSATION**

The total compensation payable to **Consultant** by **City** for the Services described in paragraph 2 shall not exceed the sums described in the attached proposal, and shall be earned on the basis as indicated in the **Consultant's** attached proposal.

All payments shall be made within thirty (30) calendar days after execution of this *Agreement*.

EXTRA SERVICES

No other extra services are authorized by this *Agreement*.

4. **PROGRESS AND COMPLETION**

The **City** and the **Consultant** are aware that many factors outside the **Consultant's** control may affect the **Consultant's** ability to complete the Services to be provided under this *Agreement*. The **Consultant** will perform these Services with reasonable diligence and expediency consistent with sound professional practices.

5. **PERSONAL SERVICES/NO ASSIGNMENT/SUBCONTRACTOR**

This Agreement is for professional services, which are personal services to the **City**. The following persons are deemed to be a key member(s) of or employee(s) of the **Consultant's** team, and shall be directly involved in performing or assisting in the performance of this work.

- Abby Albrecht, Granite Construction and Utah Transportation Coalition
- Justin Jones, Salt Lake Chamber of Commerce
- Cameron Diehl, Utah League of Cities and Towns
- Lincoln Shurtz, Utah Association of Counties

The **Consultant** will subcontract the following portions of the work out to other parties:

- Penna Powers: strategic communications, public relations, and consulting services.
- Other coalition partners

This *Agreement* is not assignable by **Consultant** without the **City's** prior written consent.

6. **HOLD HARMLESS AND INSURANCE**

Consultant shall defend, indemnify and hold the **City**, its elected Officials, officers, and employees, harmless from all claims, lawsuits, demands, judgments or liability including, but not limited to general liability, automobile and professional errors and omissions liability, arising out of, directly or indirectly, the negligent performance, or any negligent omission of the **Consultant** in performing the services described.

Consultant shall, at **Consultant's** sole cost and expense and throughout the term of this *Agreement* and any extensions thereof, carry:

- (1) Workers compensation insurance adequate to protect Consultant from claims under workers compensation acts.
- (2) Professional errors and omissions insurance in the amount of \$2,000,000, and
- (3) General personal injury and property damage liability insurance and automobile liability insurance with liability limits of not less than \$2,000,000 each claimant and \$2,000,000 each occurrence for the injury or death of person or persons and property damage.

All insurance policies shall be issued by a financially responsible company or companies authorized to do business in the State of Utah.

7. **RELATIONSHIP OF THE PARTIES**

The relationship of the parties to this *Agreement* shall be that of independent contractors and that in no event shall **Consultant** be considered an officer, agent, servant, or employee of **City**. The **Consultant** shall be solely responsible for any workers compensation, withholding taxes, unemployment insurance and any other employer obligations associated with the described work.

8. **TERMINATION BY CITY**

The **City**, by notifying **Consultant** in writing, may upon ten (10) calendar days notice, terminate any portion or all of the services agreed to be performed under this *Agreement*.

9. **WAIVER/REMEDIES**

Failure by a party to insist upon the strict performance of any of the provisions of this *Agreement* by the other party, irrespective of the length of time for which such failure continues, shall not constitute a waiver of such party's right to demand strict compliance by such other party in the future. No waiver by a party of a default or breach of the other party shall be effective or binding upon such party unless made in writing by such party, and no such waiver shall be implied from any omission by a party to take any action with respect to such default or breach. No express written waiver of a specified default or breach shall affect any other default or breach, or cover any other period of time, other than any default or breach and/or period of time specified. All of the remedies permitted or available to a party under this *Agreement* or at law or in equity shall be cumulative and alternative, and invocation of any such right or remedy shall not constitute a waiver or election of remedies with respect to any other permitted or available right or remedy.

10. **CONSTRUCTION OF LANGUAGE**

The provisions of this *Agreement* shall be construed as a whole according to its common meaning and purpose of providing a public benefit and not strictly for or against any party. It shall be construed consistent with the provisions hereof, in order to achieve the objectives and purposes of the parties. Wherever required by the context, the singular shall include the plural and vice versa, and the masculine gender shall include the feminine or neutral genders and vice versa.

11. **MITIGATION OF DAMAGES**

In all situations arising out of this *Agreement*, the parties shall attempt to avoid and minimize the damages resulting from the conduct of the other party.

12. **GOVERNING LAW**

This *Agreement*, and the rights and obligations of the parties, shall be governed and interpreted in accordance with the laws of the State of Utah.

13. **CAPTIONS**

The captions or headings in the *Agreement* are for convenience only and in no other way define, limit or describe the scope or intent of any provision or section of the *Agreement*.

14. **AUTHORIZATION**

Each party has expressly authorized the execution of this *Agreement* on its behalf and acknowledge it shall bind said party and its respective administrators, officers, directors, shareholders, divisions, subsidiaries, agents, employees, successors, assigns, principals, partners, joint ventures, insurance carriers and any others who may claim through it to this *Agreement*.

15. **ENTIRE AGREEMENT BETWEEN PARTIES**

Except for **Consultant's** proposals and submitted representations for obtaining this *Agreement*, this *Agreement* supersedes any other *Agreements*, either oral or writing, between the parties hereto with respect to the rendering of services, and contains all of the covenants and *Agreements* between the parties with respect to said services. Any modifications of this *Agreement* will be effective only if it is in writing and signed by the party to be charged.

16. **SEVERABILITY**

If any provision in this *Agreement* is held by a court of competent jurisdiction to be invalid, void or unenforceable, the remaining provisions will nevertheless continue in full force without being impaired or invalidated in any way.

17. **NOTICES**

Any notice required to be given hereunder shall be deemed to have been given by depositing said notice in this United State mail, postage prepaid, and addressed as follows:

TO CITY: _____ City
Street Address
City, Utah ZIP
Attention: City Recorder

TO CONSULTANT: Utah Transportation Coalition
c/o Salt Lake Chamber of Commerce
175 East 400 South, Suite #600
Salt Lake City, Utah 84

18. **ADDITIONAL TERMS/CONDITIONS**

Additional terms and conditions of this *Agreement* are:

IN CONCURRENCE AND WITNESS WHEREOF, THIS AGREEMENT HAS BEEN EXECUTED BY THE PARTIES EFFECTIVE ON THE DATE AND YEAR FIRST WRITTEN ABOVE.

_____ CITY:

Attest

Signature

City Recorder

Print Name

Approved as to Form

Date

Municipal Legal Counsel

CONSULTANT:



Signature

Lane Beattie, President and Chief Executive Officer

Date

State of Utah)

:ss

County of Salt Lake)

On this _____ day of _____, 2014, personally appeared before me

_____ [name of person(s)], whose identity is personally known to me or proved to me on the basis of satisfactory evidence, and who affirmed that he/she is the President and Chief Executive Officer [title], of The Salt Lake Chamber of Commerce [name of corporation], a corporation, and said document was signed by him/her in behalf of said corporation by authority of its bylaws or of a Resolution of its Board of Directors, and he/she acknowledged to me that said corporation executed the same.

Notary Public

AMENDMENT NO. 7
to the
AGREEMENT FOR ENGINEERING AND TECHNICAL SERVICES

WORK RELEASE NO. 2013-1

**PAROWAN CITY CENTER CREEK HYDROELECTRIC PLANT & PENSTOCK
PROJECT PRELIMINARY ENGINEERING SERVICES**

for

PAROWAN CITY

December 11, 2014

Whereas PAROWAN CITY (CLIENT) and SUNRISE ENGINEERING, INC. (ENGINEER) entered into an AGREEMENT FOR ENGINEERING AND TECHNICAL SERVICES, and WORK RELEASE NO. 2013-1 PAROWAN CITY CENTER CREEK HYDROELECTRIC PLANT & PENSTOCK PROJECT DESIGN & CONSTRUCTION ENGINEERING SERVICES (AGREEMENT) on the 8th day of May, 2013, and whereas the parties mutually agree to make modifications to the Agreement, more specifically to revise the Scope of Services of the Agreement, and to revise the compensation paid to ENGINEER, CLIENT and ENGINEER agree to the following modifications:

JUSTIFICATION:

The current budget for Item 2 – FERC License Consultation Services has been expended and shall be adjusted in order to continue with the required FERC License Amendment Consultation Services, to include the FERC construction phase and post construction phase services that were described in the FERC Order dated November 4, 2014 and the FERC construction authorization letter dated November 6, 2014.

COMPENSATION:

CLIENT agrees to compensate ENGINEER for services as follows and which payments shall be considered complete compensation for all engineering services outlined in the respective Articles of this Work Release.

1. For Item 2 – FERC License Consultation Services, the compensation shall be increased from \$51,000 to \$65,000. Compensation will continue to be paid for actual charges at hourly rates plus direct expenses as shown on Exhibit A.

Compensation under this Item shall be payable monthly based on prior month's services, and is due and payable within (30) thirty days.

This amendment is subject to the terms and conditions outlined in the original agreement.

IN WITNESS WHEREOF, the parties hereto have executed, by their duly authorized officials, this Amendment on the dates indicated below:

CLIENT: PAROWAN CITY

ENGINEER: SUNRISE ENGINEERING, INC.

By: _____

By:  _____

Name: Donald G. Landes

Name: Derek Anderson, P.E

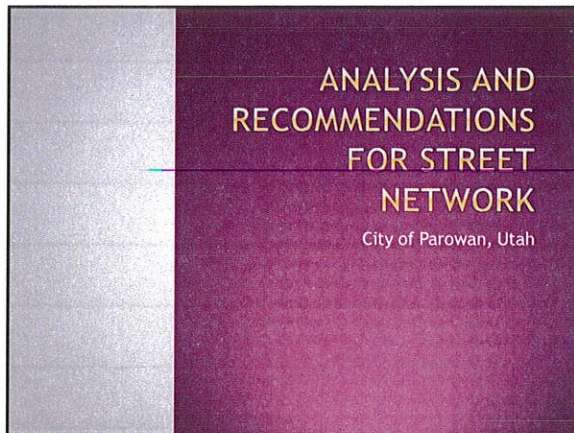
Title: Mayor

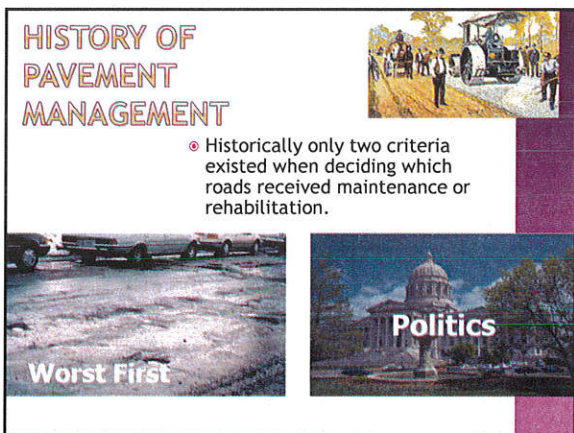
Title: Principal Engineer

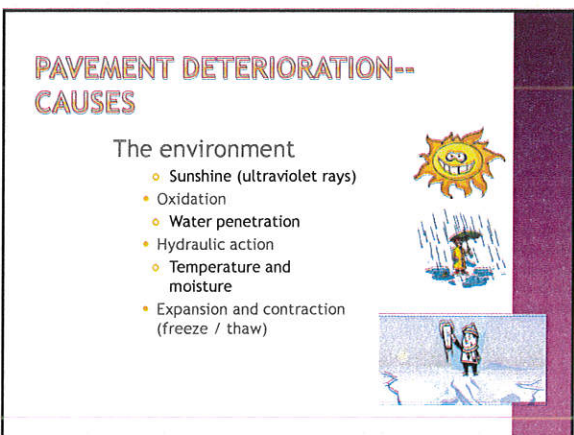
Date: _____

Date: 12/11/2014

1/6/2015

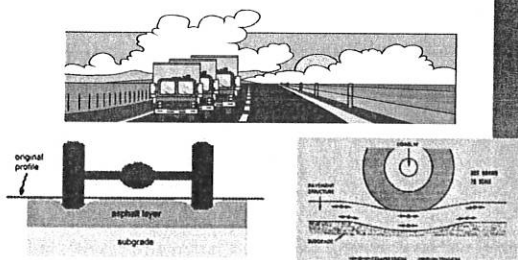






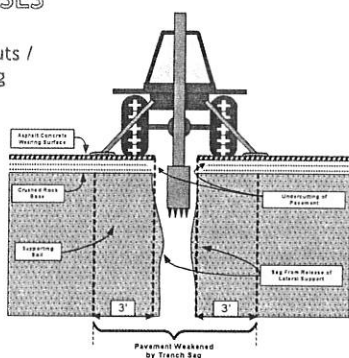
PAVEMENT DETERIORATION-- CAUSES

• Traffic Loading



PAVEMENT DETERIORATION-- CAUSES

Utility Cuts / Trenching





PAVEMENT PRESERVATION

◎ Preserve vs. Reconstruct (worst first)

- Years of pavement condition data show it is **more economical to preserve** roads than to delay repairs and reconstruct roads.
- Studies also show as traffic levels increases, the costs of delaying repair work increase significantly.
- Repairing the **worst roads first** is a **very expensive** way to operate a highway system.

KEEP YOUR GOOD ROADS GOOD

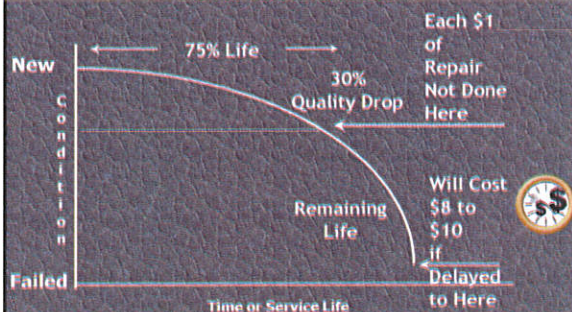


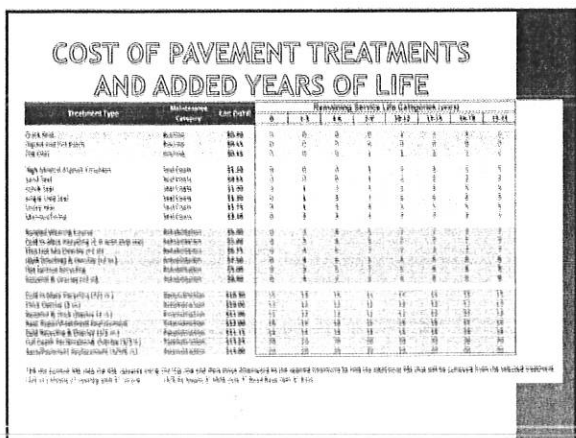
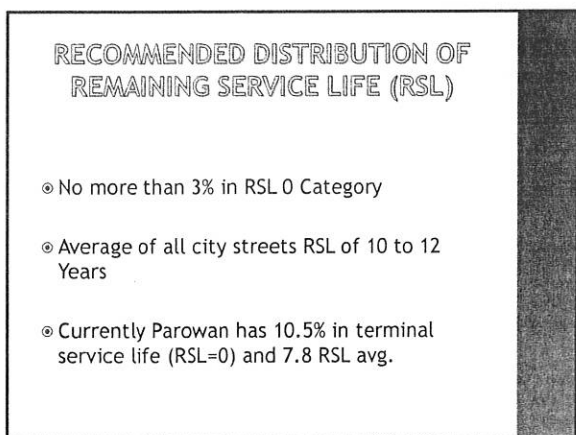
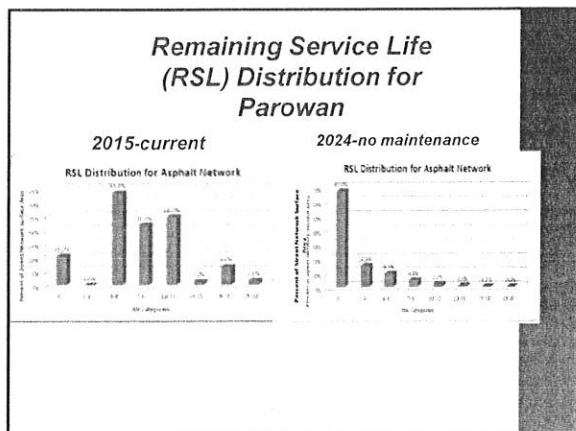
DO THESE FIRST (RSL 12)



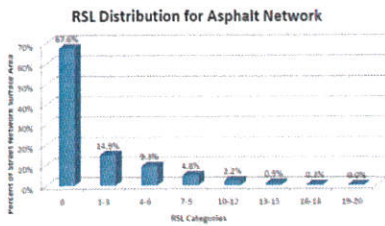
DO THESE LAST (RSL 0)

THE COST OF PAVEMENT DETERIORATION





**Remaining Service Life (RSL)
Distribution in 2024 if no
maintenance is done**

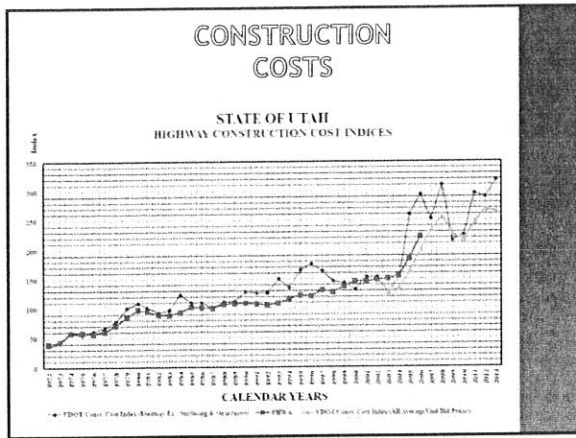


**THIS IS A ROAD WITH 0 YEARS
OF REMAINING SERVICE LIFE
(RSL) 300 S.**



**THIS IS A ROAD WITH 12 YEARS
OF REMAINING SERVICE LIFE
(RSL 12) 600 W.**





B & C FUNDS HAVE ACTUALLY DECREASED OVER THE SAME TIME FRAME

2003 through 2009

So you can only resurface half of the roads
today than in 2003 with the same money

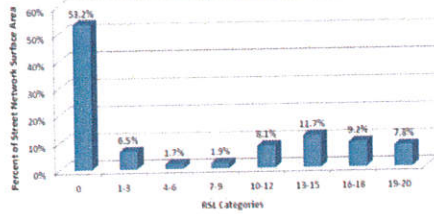
PAROWAN CITY ROAD MILEAGE AND B & C FUNDS RECEIVED

- ◎ PAVED ROADS = 28.92 miles
- ◎ GRAVEL ROADS = 4.02 miles
- ◎ DIRT ROADS = 1.31 miles

- ◎ Current B & C funds/year = \$145,074.85
- ◎ Funds Allocated per mile = \$5016.42

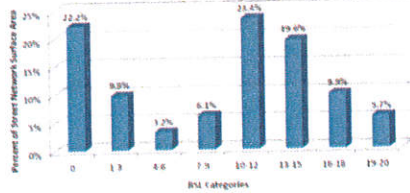
2024 - CONTINUE WITH CURRENT ALLOCATION OF \$94,000

RSL Distribution for Asphalt Network



RECOMMENDED PRESERVATION PROGRAM OF \$335,200 AVERAGE PER YEAR 2014 - 2018

RSL Distribution for Asphalt Network



Paved Road Funding Distribution for 2014-2018


Pavement Preservation Strategies	Percent of Street Network	Funding Distribution
Routine Maintenance	3.0%	\$1,800
Preventive Maintenance	10.0%	\$45,200
Rehabilitation	6.0%	\$286,300
Reconstruction	0.0%	\$0
TOTAL	19.0%	\$335,200

Paved Road Funding Distribution for 2019 - 2024

Pavement Preservation Strategies	Percent of Street Network	Funding Distribution
Routine Maintenance	2.5%	\$3,100
Preventive Maintenance	4.0%	\$18,100
Rehabilitation	1.0%	\$47,700
Reconstruction	5.8%	\$179,300
TOTAL	12.8%	\$248,200

PAVEMENT PRESERVATION THE TREATMENTS


*TRACK
THE PAVEMENT
CRACKS*




PAVEMENT CORRECTION

- Requires extensive surface preparation
 - Plug patching and localized base repairs
 - Localized milling or grinding
 - Crack sealing


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
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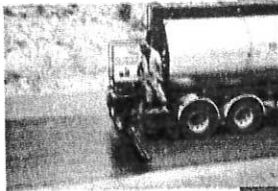
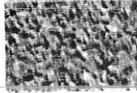


PAVEMENT PRESERVATION THE TREATMENTS

- Fog Seal
 - Diluted asphalt emulsion
 - Protects against environmental impacts
 - Prevents ravel

High Mineral Content Asphalt Emulsion.
Protects from UV weathering

PRESERVATION SEALS THE TREATMENTS

• Slurry Seal

- Includes asphalt emulsion, aggregate, latex fibers
- Protects asphalt surface against oxidation and ravel.
- Restores skid resistance, seals emerging cracks, improves appearance.



PRESERVATION SEALS THE TREATMENTS

• Chip Seal

- Aggregate evenly spread and embedded onto emulsion
- Protects asphalt surface against oxidation and ravel.
- Restores skid resistance, seals emerging cracks, improves appearance.



PRESERVATION SEALS THE TREATMENTS

• Cape Seal

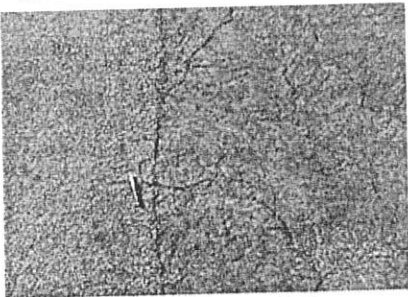
- Includes chip seal followed by a slurry seal.
- Protects asphalt surface against oxidation and ravel.
- Restores skid resistance, seals emerging cracks, improves appearance.



+



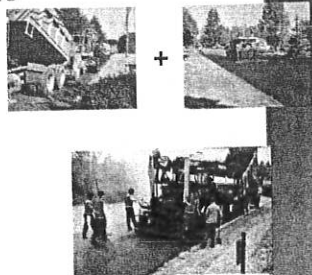
PAVEMENT PRESERVATION
THE TREATMENTS



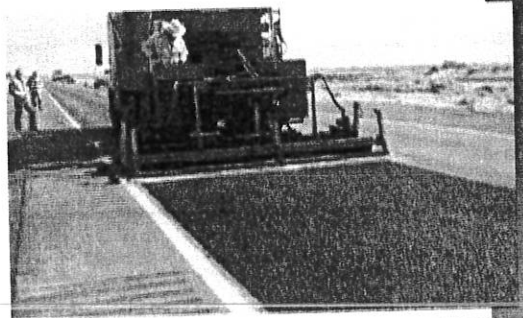
Cape Slurry Only

PAVEMENT PRESERVATION
THE TREATMENTS

- Chip Seal
 - Single or double layer
- Cape Seal
- Thin Overlay



FLEXIBLE MICRO SURFACING



BONDED WEARING COURSE



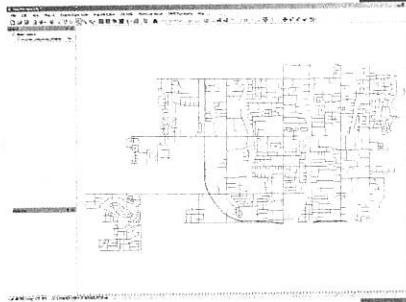
FULL DEPTH RECLAMATION



COST OF PAVEMENT TREATMENTS AND ADDED YEARS OF LIFE

[illegible][illegible]

GIS Based Pavement Management System

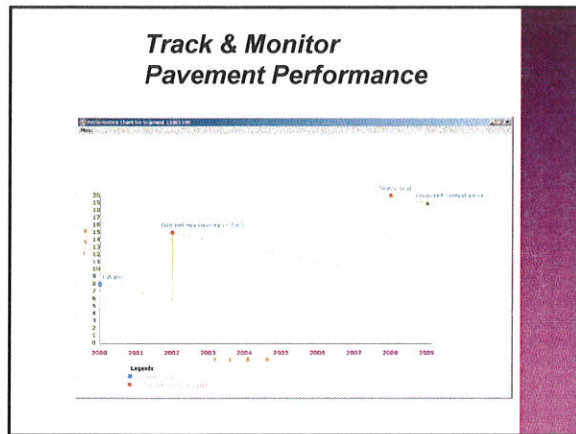


Pavement Evaluation Form

Edit Inventory Information		Distress Rating Sheet	
Segment Number	100152	Fatigue	0/8
Road Name	10005	Longitudinal	0/8
From Address	10000	Transverse	0/8
To Address	10000	Block	0/8
Number of Travel Lanes	2	Paving Patches	0/8
Road Width	10	Edge	0/8
Segment Length	100	Routing	0/8
Segment Label	100	Reinforced	0/8
Surface Type	Asphalt	Drainage	0/8
Curve	0	Inventory Date	1/6/2015
Importance	Medium-High	PSI	
Functional Classification	Major Arterial	Optimal Treatment	
Unit	Feet		
Drainage Type	Stormwater		
ADT	100		
Date Inspected	1/6/2015		
Photo #	100		
Update Location Information			
View Picture			
Enter Comment			

Benefits of a pavement preservation system:

- Updated records of pavement condition
- Track pavement performance
- Budget allocation





Analysis and Recommendations for Street Network

Parowan City



December 2014

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Appendix B. Condition Survey Evaluation Sheet

Appendix C. Condition Survey of Street Network

Appendix D. Distress Deterioration Table and Recommended Preservation Strategies

Appendix E. Recommended Preservation Strategies for Each Street Segment

Appendix F. Preservation Strategies, Treatments, and Associated Costs

Appendix G. Analysis of Current Pavement Preservation Program

Appendix H. Recommended Pavement Preservation Program and Proposed Funding Allocation

Introduction

One of Parowan's most valuable infrastructure assets is the 30± miles of local streets within its network. Maintaining the street network at a high level of service will promote the prosperity of Parowan's entire community. Many state and local transportation agencies currently use a pavement management system and/or a maintenance management system to cost effectively preserve and improve their street network. The Utah Local Technical Assistance Program (LTAP) assists local agencies in the state of Utah and surrounding states to implement and use such a tool to maintain, preserve, and enhance their road and street facilities and more effectively manage the allocation of funding as it pertains to the existing street network.

The City of Parowan asked the Utah Local Technical Assistance Program (LTAP) to develop a pavement management system that could be used in their transportation plan. This report describes the system's major elements, the processes, and the work accomplished to facilitate its implementation in Parowan. The pavement management system provides:

- A complete GIS-based physical inventory and condition survey of the street network
- A needs assessment process
- Analyses of root causes of pavement deterioration
- Analysis of current street maintenance programs
- Recommended maintenance and preservation treatments
- Treatment costs and budget proposals
- A method to evaluate alternate funding scenarios to maximize the average remaining service life (RSL) of the street network

Figure 1 outlines the major elements and processes incorporated in Parowan's pavement management system. The following sections of this report describe each step of the process in detail, the results of field surveys and analyses, and the conclusions and recommendations offered to assist in the full implementation of the system in Parowan.

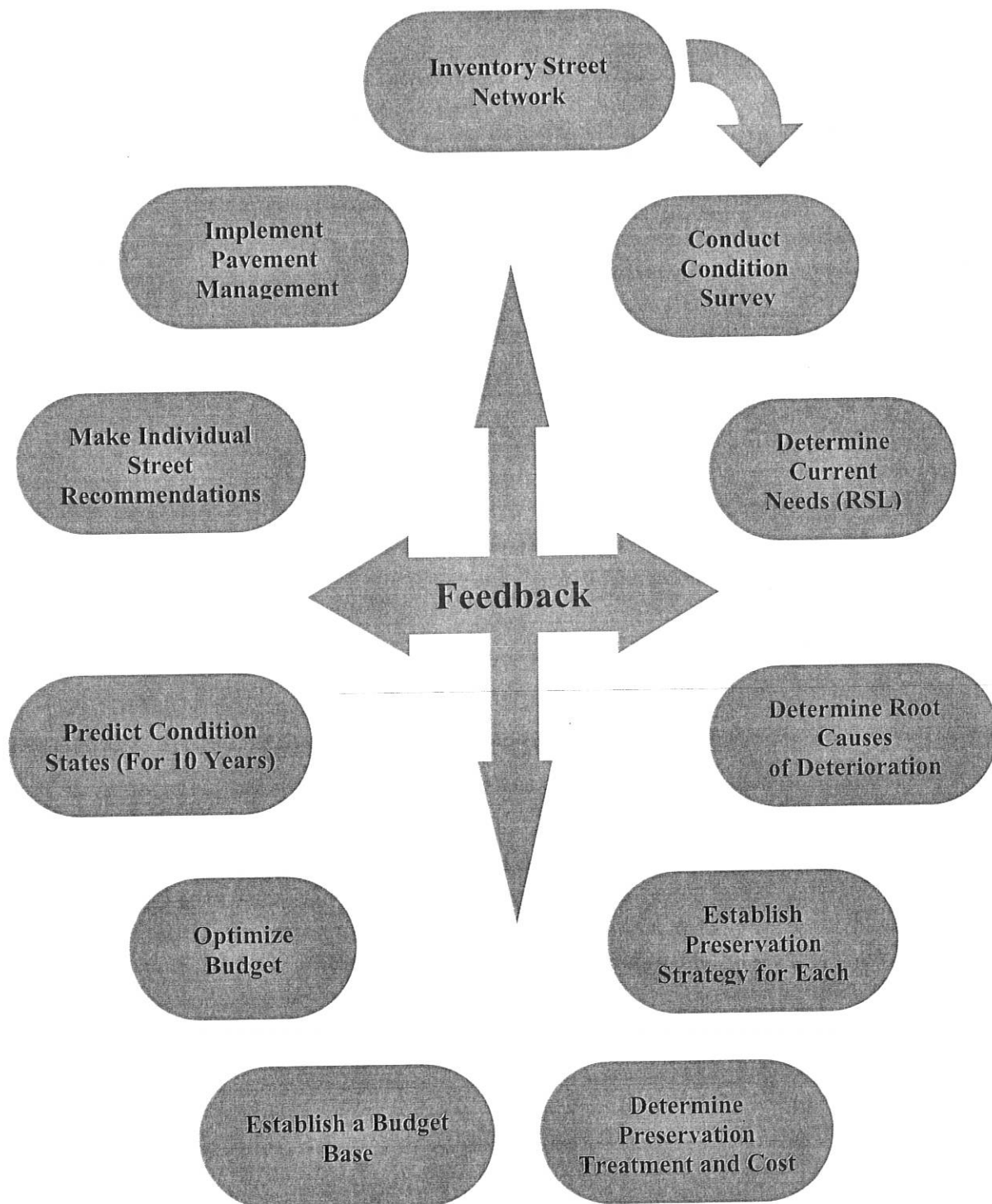


Figure 1. Pavement Management Process Diagram

Inventory of Road Network

The first step in the process of inventorying Parowan's local street network involved assigning a functional classification to each street. City officials assisted in making these classifications. Excluding the state routes, the inventory identified three functional classifications: residential, minor collector, and minor arterial. Currently, the Utah Department of Transportation (UDOT) maintains all sections of Main Street, Canyon Road, and 2nd South within city boundaries.

Parowan road network GIS maps have been maintained by the State, which served as a base map for the inventory. In addition to using the existing shapefiles of the centerlines of each street, a measuring wheel was used to measure the widths. The State-maintained GIS data was used to calculate the lengths of all street segments. These measured widths and lengths were used to calculate the surface areas.

A complete condition survey of Parowan's local road network was conducted during November of 2014. Employees from the Utah LTAP (Local Technical Assistance Program) Center used the Strategic Highway Research Program (SHRP) Distress Manual as a guide to conduct the pavement distress survey.

Appendix A has the complete results of the inventory processes. Inventory details include street name, starting and ending addresses of the segment, functional classification, segment width and length, estimated remaining service life (RSL), surface area of the pavement in square yards, and the percent of network area represented by each segment.

Table 1, an excerpt from Appendix A, shows the details covered in the inventory process.

Table 1. Excerpt Showing Details in the Inventory Process of the Local Street Network

ID	Street Name	From	To	Class	Width (ft)	Length (ft)	RSL	%Area	Area (yd ²)
117	100 S	200 E	300 E	Residential	20	563	20	0.25%	1251
205	200 S	200 E	300 E	Residential	30	556	12	0.37%	1853
292	1600 W	725 S	Old HWY 91	Residential	37	226	12	0.18%	929
40	300 E	300 S	200 S	Residential	34	468	10	0.35%	1768
20	Old HWY 91	1050 W	850 W	Minor Arterial	28	1383	6	0.85%	4303
341	N. Airport Rd.	50 E	END OF PAVEMENT	Residential	25	1494	0	0.82%	4150
263	750 W	END PAVEMENT	Old HWY	Residential	42	1129	2	1.04%	5269

Table 2 provides a summary of the street inventory information in terms of surface area and the percent of the street network represented by each functional class.

Table 2. Functional Classification by Surface Area and Percent of Local Street Network

	Minor Arterial	Minor Collector	Residential	Total
Area (yd ²)	29,589	9,247	465,624	504,460
Percent of Road Network	5.87%	1.83%	92.30%	100.00%

As shown in Table 2, the street network in Parowan is classified as 5.9% minor arterial, 1.8% minor collector, and 92.3% residential. Figure 2 illustrates this information in graphical form.

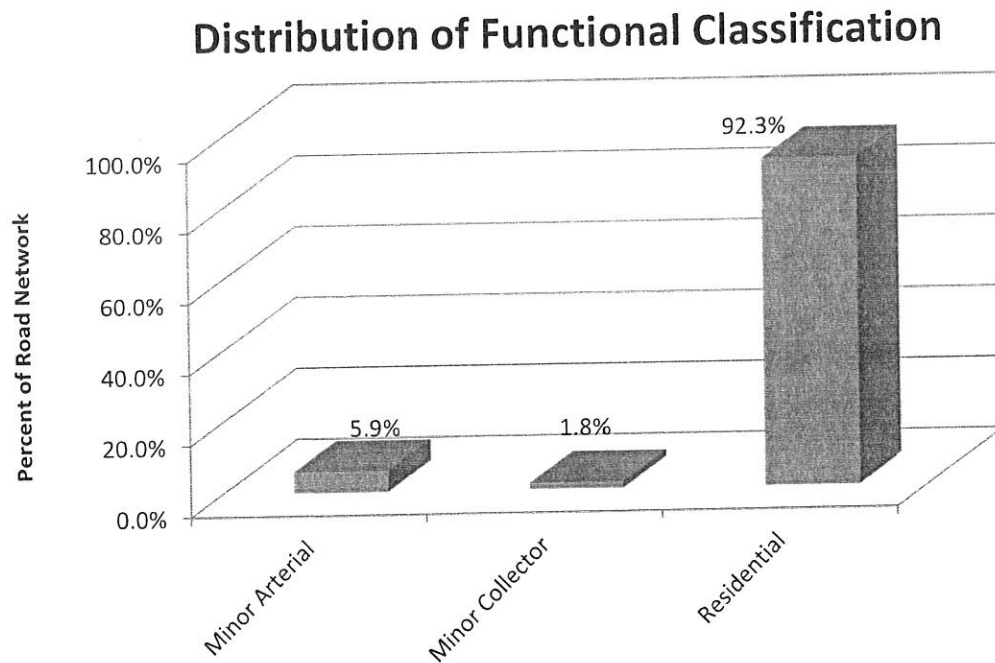


Figure 2. Distribution of Street Network by Functional Classification

This inventory excludes pavement structure details such as date of initial construction, layer thickness, and pavement design criteria of each street. This information can be obtained from historical records, maintenance personnel, or sampling and testing of the pavement structure. This information should be incorporated through further implementation efforts and by working closely with City of Parowan Public Works.

Pavement Condition Survey

Asphalt Road Network

A complete condition survey covering surface smoothness, drainage, and pavement distress of Parowan's road network was conducted during November 2014. Employees from the Utah LTAP (Local Technical Assistance Program) Center used the Strategic Highway Research Program (SHRP) manual, Distress Identification Manual for the Long-Term Pavement Performance Project as a guide to conduct the pavement distress survey.

The principal focus of the condition survey was to identify and determine the severity level and extent of each distress type. Each asphalt street segment was closely surveyed with respect to potholes/utility cuts, rutting, transverse cracking, longitudinal cracking, block cracking, edge cracking, and fatigue (alligator) cracking. The severity level and extent of each distress type were evaluated in accord with the condition survey evaluation sheet shown in Appendix B. Appendix C shows the detailed distress information for each road segment.

Pavement surface smoothness and surface drainage for each segment were evaluated subjectively using the ratings of excellent, good, fair, or poor. Pavement surface smoothness of each street was determined by driving over each segment. If the pavement appeared to be new and there was no discernible roughness felt in the ride of the vehicle, the pavement smoothness was rated excellent. The pavement smoothness was rated good if the ride of the vehicle remained smooth, yet the pavement appeared to show some signs of distress. A rating of fair for pavement smoothness was given if there was notable roughness felt in the ride of the vehicle but little discomfort sensed by the driver. If there was substantial roughness felt in the ride of the vehicle, leading to a reduction of speed of the vehicle below the posted limit, the pavement smoothness was rated as poor. Table 3 summarizes these criteria.

Table 3. Pavement Surface Smoothness Rating Criteria

RATINGS	SMOOTHNESS RATING CRITERIA
Excellent	Newly Constructed
Good	Smooth ride, minor distress
Fair	Moderate Distress
Poor	Severe Distress (decrease speed)

Figure 3 shows the results of the pavement smoothness survey.

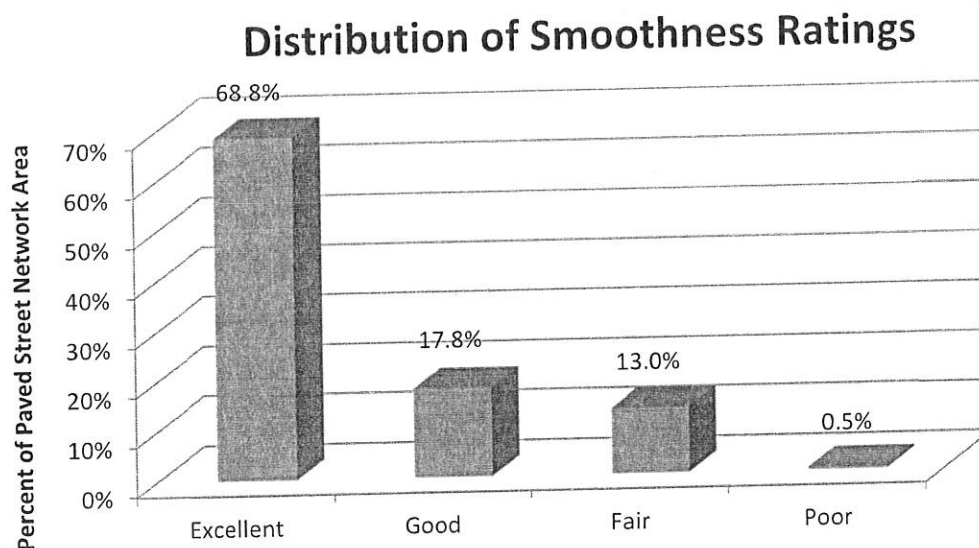


Figure 3. Distribution of Smoothness Ratings

As shown in the figure above, the majority of Parowan's asphalt streets under the "Excellent" category. Rough streets provide a lower level of service that may increase car repair costs and fuel consumption.

During the condition survey, pavement surface drainage conditions were also evaluated. Table 4 shows the rating criteria used to evaluate pavement surface drainage.

Table 4. Pavement Surface Drainage Rating Criteria

RATINGS	DRAINAGE RATING CRITERIA
Excellent	Newly constructed, cross-slope > 2%, drainage provisions provided
Good	Cross-slope > 2%, drainage provisions provided
Fair	Cross-slope < 2%, no drainage provisions provided
Poor	Flat or concave cross-slope, ponding surface water evident, no drainage provisions provided

Visual condition surveys were calculated to evaluate pavement surface drainage conditions. If a newly-constructed road had a cross-slope of at least two percent (2%) or more, and provision was made for surface water to drain (e.g., well-graded ditches, curb and gutter, drainage

structures, or paved ditches); surface drainage was rated as excellent. In the case that the road was not newly constructed but otherwise met the preceding criteria then it received a surface drainage rating of good. When the pavement cross-slope varied below two percent (2%), evidence of surface water ponding was observed, or the surface drainage features were not well maintained; surface drainage conditions were rated as fair. A poor surface drainage rating was given if the pavement cross-slope was flat or directed toward the centerline of the street, if surface water ponding was observed, or drainage provisions were not maintained or provided. The following four photographs illustrate each of these ratings:



Photo 1. Excellent Surface Drainage – 100 North from 200 West to 100 West

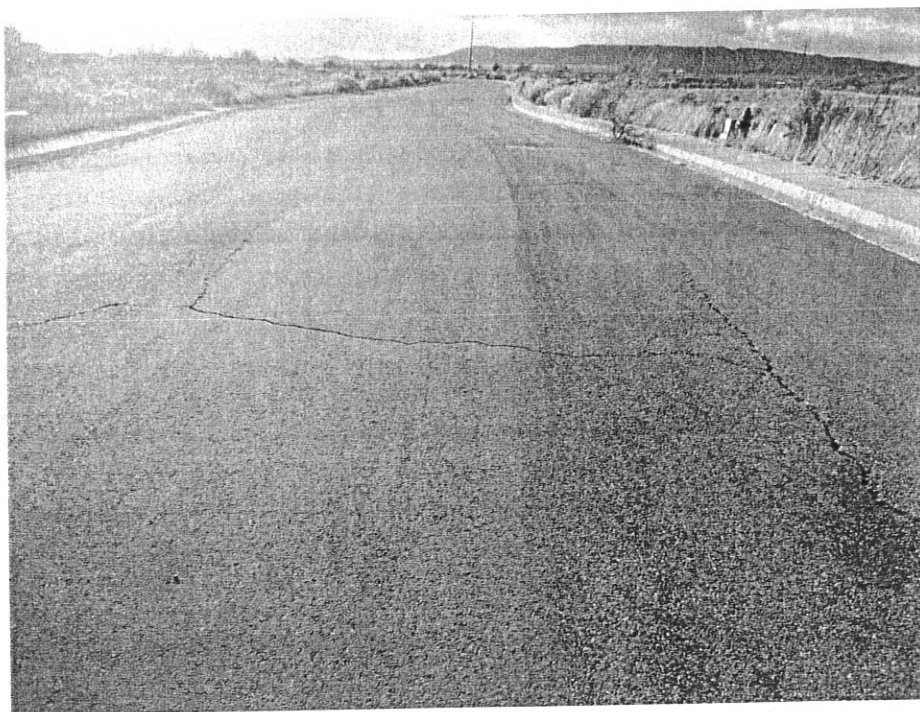


Photo 2. Good Surface Drainage – 560 West from 300 North to DEAD END



Photo 3. Fair Surface Drainage – 500 North from 500 West to Diane Drive



Photo 4. Poor Surface Drainage- 900 West from DEAD END to 200 North

Figure 4 shows the pavement drainage rating distribution for Parowan's street network in graphical form.

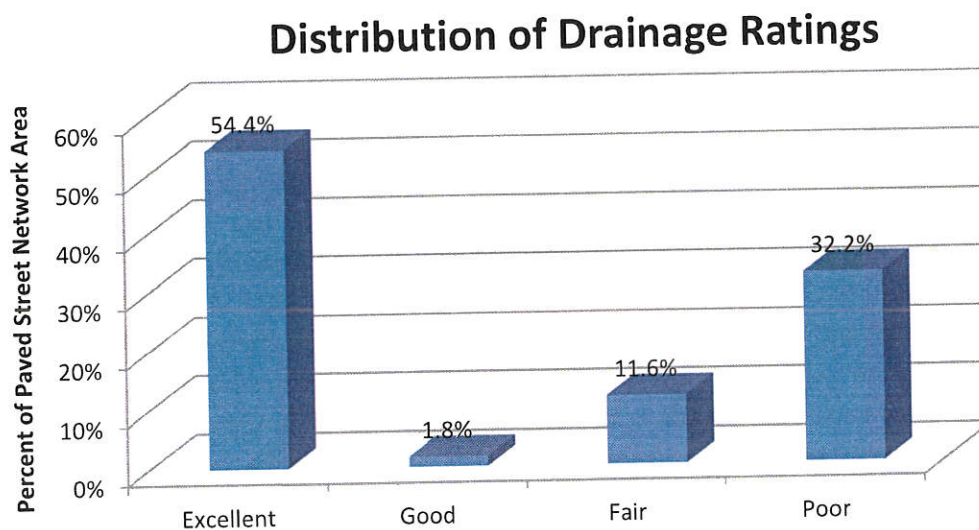


Figure 4. Pavement Drainage Rating Distribution

Asphalt Pavement Design & Performance

Typically, asphalt pavements designed in accord with the AASHTO Guide for Design of Pavement Structures, ought to provide for twenty years of traffic loading (18 kip ESAL's) before reaching a terminal serviceability level at which point reconstruction is required (RSL = 0). For management purposes, the same estimate is used to calculate the service life of concrete pavements as well. Conventional practice usually provides for a preventative maintenance treatment and rehabilitative treatment to be applied to the asphalt or concrete pavement during its 20-year service life. Timing is critical in the placement of the preventative maintenance and the rehabilitative treatment to achieve the best level of service at the least amount of cost.

Figure 5 shows a typical pavement performance curve for asphalt pavements. This figure emphasizes the time relationship between street pavement condition and the cost of repair.

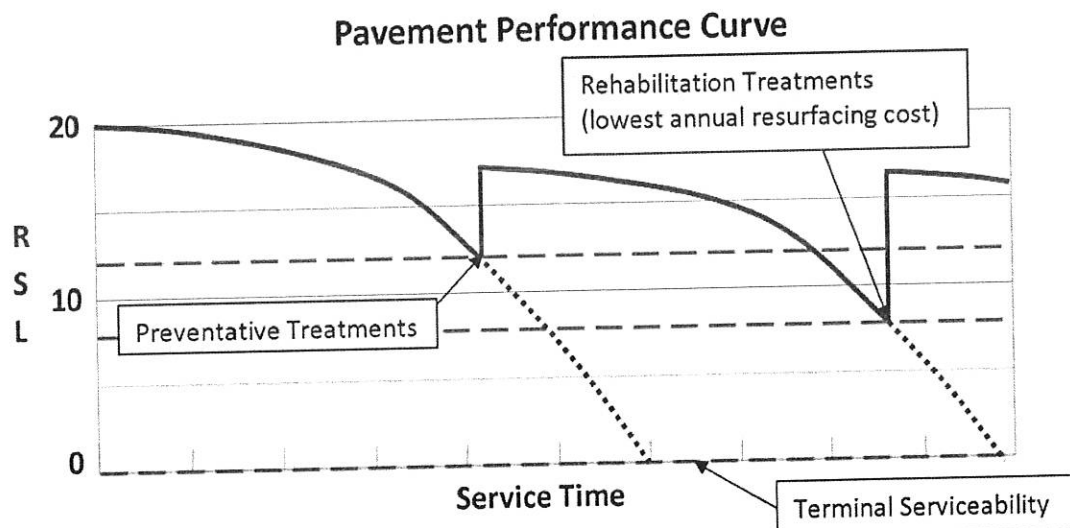


Figure 5. Pavement Performance Curve

After eight years of service (RSL = 12), most asphalt and concrete pavements will deteriorate to a "good" condition category. This relates to a thirty-three percent (33%) drop in the service life of the pavement and is the optimal point in time at which a preventative maintenance treatment should be placed. After twelve years of service (RSL = 8), most asphalt and concrete pavements will deteriorate to a "fair" condition rating. This represents a sixty percent (60%) drop in the service life of the pavement and is the best point in time at which to consider a rehabilitation treatment. If no renovation action occurs at this point, the street will likely deteriorate to the "poor" category within three years (RSL = 5). Cost comparisons show that reconstruction costs three to five times more than rehabilitation strategies. The cost to maintain a pavement with preventative maintenance strategies relates to about one-third the cost of rehabilitation strategies, or one-sixth the cost of reconstruction.

Major Causes of Asphalt Pavement Distress

The predominant asphalt pavement distresses affecting Parowan's streets were determined from the pavement distress survey information. Analysis of this information showed that there were seven major distress types in the street network. Pavement roughness results from these distresses. Fatigue cracking was the major distress type found occurring most frequently in the street network.

The root causes of each of the seven main distress types are described as follows, along with respective suggestions on how to mitigate the development of each:

Transverse cracking in asphalt pavements is normally attributed to thermal changes in the pavement structure. As seasonal temperatures change, the pavement expands and contracts beyond the limits that asphalt can tolerate, thus causing transverse cracking. If these transverse cracks are not sealed early in their development, they will continue to grow in terms of both severity and extent, and they will allow surface moisture to enter the pavement causing further distress to develop. Recent developments in asphalt technology known as the Superpave System have shown the potential to preclude the development of transverse cracking if used in new asphalt pavements. Use of performance graded (PG) asphalt cements and the Superpave mix design system, along with good quality control and good hot mix asphalt construction practice can potentially eliminate this type of distress from occurring. Using the Superpave System on newly constructed or reconstructed streets that serve a relatively high volume of traffic is recommended.

Longitudinal cracking is related to two different causes. The first is poor construction. When a street is constructed, it is normally built in two or more sections. Problems, such as poor compaction or segregation in the asphalt mix, will cause longitudinal cracks along the construction seam. The second cause of longitudinal cracks is load related. These longitudinal cracks are found in the wheel paths of the travel lanes. These cracks are due to early fatigue failure and should be treated as fatigue cracks. On some street segments that are extremely wide, longitudinal cracking may be caused by thermal changes as with transverse cracks.

Block cracking is a combination of transverse and longitudinal cracking that occurs when the transverse and longitudinal cracks intersect. The combination of these two distresses allows greater opportunity for surface water to enter the pavement structure, thus decreasing the load carrying capacity of the pavement. Once a block forms, water enters and softens the base. As the base softens, normal traffic loading progressively breaks the pavement into smaller and smaller blocks. This leads to the development of fatigue cracking.

Utility cuts are man-made cuts and have been shown to reduce the service life of a street by as much as five to seven years. Although utility cuts are sometimes inevitable, good planning and coordination of utility work can reduce the number of utility cuts made in newer streets.

Only limited rutting of the pavement surface was observed in Parowan's street network. This form of distress typically occurs in the wheel paths and is a result of deformation in the pavement structure or subgrade. This deformation comes from heavy axle loads acting in combination with moisture to deform and rut the pavement. Inadequate compaction during construction can also result in deformation. Rutting may also occur in hot weather when the asphalt is less viscous and has less shear strength. In this case, rutting usually results from the use of poor materials, poor asphalt mix design, poor quality control, or poor construction.

Edge cracking was generally found in street segments where pavement edges had little or no support. Those segments that had no paved shoulders or supporting curb and gutter sections were more prone to this type of distress.

Fatigue cracking in asphalt pavements is largely caused by loss of base and subgrade support due to moisture infiltrating the pavement. Once moisture softens the base and subgrade layers, the asphalt pavement can no longer effectively carry the traffic loading. This results in pavement cracking and breakup. The fatigue cracking prevalent in the streets of Parowan is most likely caused by water saturating the base and subgrade layers. With the subgrade saturated, the road structure flexes and gives under the weight of a vehicle that drives over the street. Heavy vehicle traffic on the streets also causes fatigue cracking. Such vehicles cause higher stresses in the pavement than is provided in the design. In those areas of the city where new homes are being constructed, concrete trucks or other heavy vehicles, can cause major damage to the streets. Heavy commercial trucks fall within the heavy vehicle traffic designation.

Pavement Distress Survey & Analysis

The first step in the analysis of the pavement distress survey information involved determining the governing distress type for each street segment. A governing distress is one that is most detrimental to the condition of the pavement, and so should be the focus of treatment. Each rating for each distress is associated with an RSL value; the worse the rating, the worse the RSL. To analyze a segment, find the lowest RSL value associated with any of the distresses assigned to the segment. This value becomes the RSL for the entire segment and the corresponding distress is the governing distress.

Figure 6 shows an example rating sheet for a road segment and Table 5 shows the RSL values associated with fatigue cracking ratings. The distress rating of 5 for fatigue cracking corresponds with an RSL of 8. Similar tables would be used for the other distresses reported on the segment. An analysis of the distresses shown below shows that fatigue cracking is the governing distress because it gives the lowest RSL value (besides being the highest numerical rating).

FATIGUE CRACKING		BLOCK CRACKING					
	Extent		Extent				
	Low Medium High		Low Medium High				
	0 None		0 None				
	1 Cracks 1/4" or 1' of C&G Length		1 Cracks 1/4" or 1' of C&G Length				
Low	1	2	3	Low	1	2	3
Medium	4	5	6	Medium	4	5	6
High	7	8	9	High	7	8	9
LONGITUDINAL CRACKING		UTILITY CUTS					
	Extent		Extent				
	Low Medium High		Low Medium High				
	0 None		0 None				
	1 Cracks 1/4" or 1' of C&G Length		1 Cracks 1/4" or 1' of C&G Length				
Low	1	2	3	Low	1	2	3
Medium	4	5	6	Medium	4	5	6
High	7	8	9	High	7	8	9
TRANSVERSE CRACKING		EDGE CRACKING					
	Extent		Extent				
	Low Medium High		Low Medium High				
	0 None		0 None				
	1 Cracks 1/4" or 1' of C&G Length		1 Cracks 1/4" or 1' of C&G Length				
Low	1	2	3	Low	1	2	3
Medium	4	5	6	Medium	4	5	6
High	7	8	9	High	7	8	9
Drainage / Roughness		Rutting					
Excellent Good Fair Poor		Excellent Low Med High					
		0 <3/8" 1/2"-3/4" >3/4"					

Figure 6. Condition Rating Sheet

Table 5. Fatigue Cracking Distress Table

RATING	SEVERITY & EXTENT	RSL
0	No Alligator Cracking	20
1	Low, Low	16
2	Low, Medium	10
3	Low, High	6
4	Medium, Low	12
5	Medium, Medium	8
6	Medium, High	4
7	High, Low	10
8	High, Medium	6
9	High, High	0

The governing distress is the distress most likely to cause the pavement to deteriorate the soonest and reduce the serviceability of the street. Appendix D contains the deterioration tables for the other distress types. These tables can be adjusted by experienced personnel to more accurately reflect the effects of local environmental and traffic loading conditions.

Table 6 includes several recommended preservation strategies and treatments, the estimated cost of each treatment, and the estimated remaining service life the road is expected to gain after the treatment is applied. The yellow band highlights the most economical treatments for a given condition category.

Table 6. Maintenance Performance Table

Treatment Type	Maint. Category	Cost	0	1-3	4-6	7-9	10-12	13-15	16-18	19-21
Crack Seal	Routine	\$0.30	0	0	0	0	1	2	3	2
Digout and Hot Patch	Routine	\$0.45	0	0	0	0	0	0	0	0
Fog Coat	Routine	\$0.45	0	0	0	1	1	2	2	2
High Mineral Asphalt Emulsion	Preventative	\$1.20	0	0	0	1	2	3	5	5
Sand Seal	Preventative	\$0.65	0	0	0	1	2	2	2	2
Scrub Seal	Preventative	\$1.00	0	1	3	4	5	5	5	5
Single Chip Seal	Preventative	\$1.30	0	1	3	4	5	5	5	5
Slurry Seal	Preventative	\$1.75	0	1	3	4	5	5	5	5
Microsurfacing	Preventative	\$2.40	0	2	3	4	7	7	7	7
Bonded Wearing Course	Rehabilitation	\$12.00**	0	3	4	5	7	7	7	7
Cold In-place Recycling (2 in with chip seal)	Rehabilitation	\$5.00	0	3	4	5	6	7	7	7
Thin Hot Mix Overlay (<2 in)	Rehabilitation	\$6.75	0	4	6	7	7	7	7	7
HMA (leveling) & Overlay (<2 in.)	Rehabilitation	\$7.50	0	4	6	8	8	8	8	8
Hot Surface Recycling	Rehabilitation	\$5.00	0	3	5	7	8	8	8	8
Rotomill & Overlay (<2 in)	Rehabilitation	\$8.40	0	4	7	8	8	8	8	8
Cold In-place Recycling (2/2 in.)	Reconstruction	\$10.30	15	15	15	15	15	15	15	15
Thick Overlay (3 in.)	Reconstruction	\$10.00	12	12	12	12	12	12	12	12
Rotomill & Thick Overlay (3 in.)	Reconstruction	\$11.00	12	12	12	12	12	12	12	12
Base Repair/Pavement Replacement	Reconstruction	\$12.00	16	16	16	16	16	16	16	16
Cold Recycling & Overlay (3/3 in.)	Reconstruction	\$11.15	14	14	14	14	14	14	14	14
Full Depth Reclamation & Overlay (3/3 in.)	Reconstruction	\$13.25	20	20	20	20	20	20	20	20
Base/Pavement Replacement (3/3/6 in.)	Reconstruction	\$19.00	20	20	20	20	20	20	20	20

*Fit the current RSL into a category along the top row and then move downward to the applied treatment to find the additional RSL that will be achieved from the selected treatment.

(2/2 in.) Means 2" overlay with 2" recycle

(3/3/6) Means 3" HMA over 3" Road Base over 6" Base

** Price of bonded wearing course quoted from <http://www.dot.ca.gov/hq/maint/MTAGChapter11-BondedWearingCourse.pdf>

The procedure outlined above was used to determine the governing distress and the RSL for each asphalt segment. Figure 7 shows the governing distress types in the asphalt street network along with the percent of the total street network area primarily affected by each type.

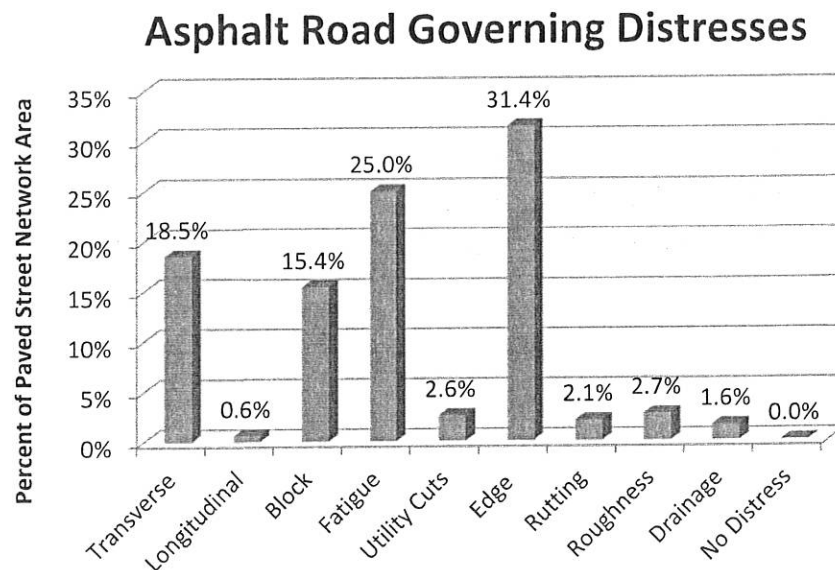


Figure 7. Governing Distress Rating Distribution for Local Asphalt Roads

As a reference, one percent (1%) of Parowan's street network represents approximately 0.34 miles in length or about 2 city blocks. Figure 7 also illustrates that some governing distress types are more common to the street network than others. Edge cracking is the most common governing distress type in Parowan's asphalt street network.

The governing distress type of each segment provided the means of calculating the average RSL for the street network. For management purposes, the estimated RSL values are grouped incrementally in three-year categories. Figure 8 shows the current RSL distribution for Parowan's street network in terms of percent of surface area of the network. The estimated average RSL of Parowan's asphalt street network is 7.8 years. This average RSL value is lower than many cities surveyed to date by the Utah LTAP Center.

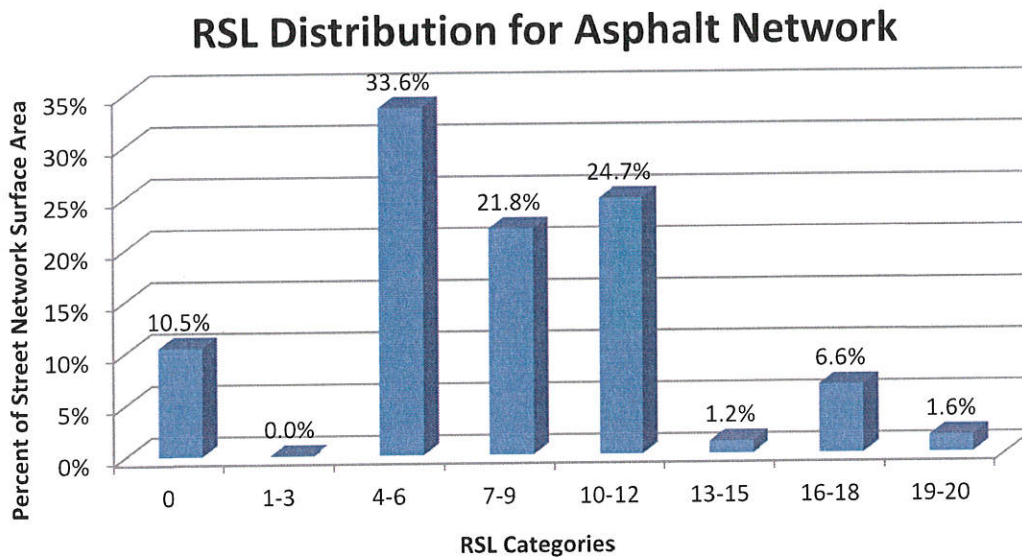


Figure 8. Current RSL Distribution for Local Asphalt Street Network

Table 7 shows this same information along with the corresponding subjective condition ratings of poor, fair, good, very good, and excellent.

Table 7. Subjective Condition Rating of Asphalt and Concrete Street Network

SUBJECTIVE CONDITION RATING OF STREET NETWORK								
	FAILED	POOR		FAIR	GOOD	VERY GOOD		EXCELLENT
RSL (Years)	0	1-3	4-6	7-9	10-12	13-15	16-18	19-20
% of Network	10.5%	0.0%	33.6%	21.8%	24.7%	1.2%	6.6%	1.6%

Eleven percent (10.5%) of the paved street network in Parowan is considered to be in a failed condition. Thirty-four percent (33.6 %) is considered to be in poor condition. Twenty-two percent (21.8%) is rated to be in fair condition, twenty-five percent (24.7%) is in good condition, eight percent (7.8%) is in very good condition, and two percent (1.6%) of the street network is rated to be in excellent condition.

For further illustrative purposes, the following photographs show examples of the condition ratings of failed, fair, good, and excellent and their respective RSL estimates.



Photo 5. Failed Condition – 300 South from 300 East to Canyon Road (RSL = 0 years)

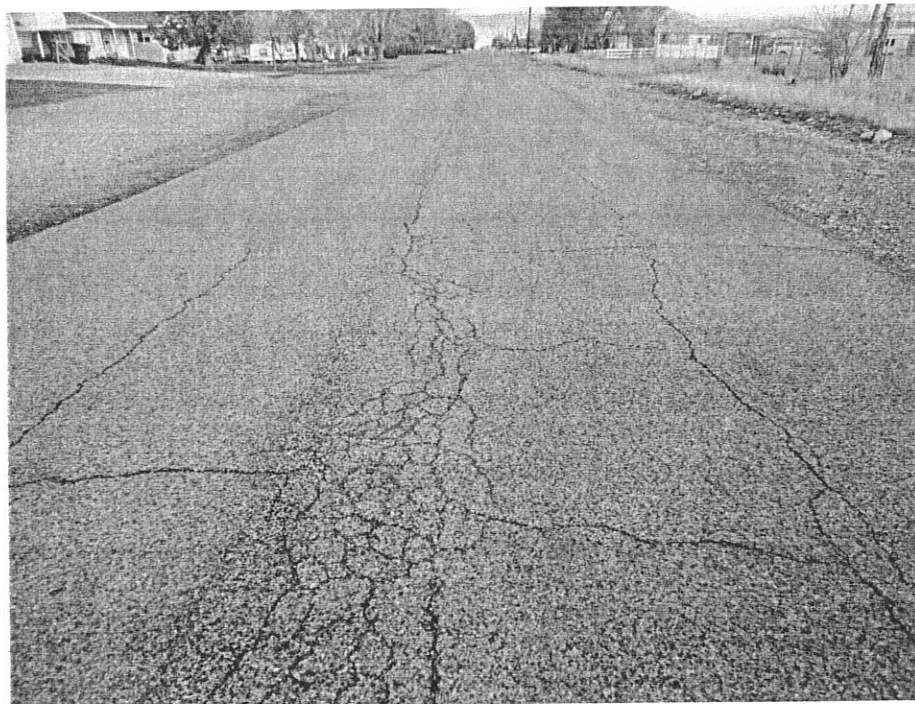


Photo 6. Fair Condition – 300 East from 100 North to 200 North (RSL = 8 years)



Photo 7. Good Condition – 600 West from 300 North to DEAD END (RSL = 12 years)



Photo 8. Excellent Condition – 950 South from 1600 West to End of Pavement (RSL = 20 years)

Currently, Parowan's paved street network is in "fair" condition. Forty-four (44.1%) of the network is in failed or poor condition as shown in Table 6. If no preservation or rehabilitation work is undertaken, an additional 21.8% can be expected to deteriorate to this level within 3 years. This could place a major financial burden on the city to reconstruct these segments to provide adequate roads, as well as reduce the amount of public content with the street network. If a systematic pavement management program is implemented now, a balanced set of preservation strategies (e.g., routine maintenance, preventative maintenance, rehabilitation, and reconstruction) can be used to preclude the development of a backlog of needs and the overall decline in the service life of the network.

Development of Preservation Strategies and Recommended Treatments

After determining the governing distress types for each street segment, pavement preservation strategies and treatments that can effectively correct or remove the root causes were identified. Frequently, more than one strategy or treatment is used to cost effectively remedy the governing distress and other accompanying distresses that may exist. As an example, the distress deterioration table for fatigue cracking is shown in Table 8. This table shows the various combinations of severity and extent (rating) levels that may occur, along with their preservation strategies and recommended treatments. The corresponding estimated RSL of each rating level is also shown.

Table 8. Fatigue Cracking Preservation Strategies and Treatments

RATING	SEVERITY & EXTENT	RSL	STRATEGY	TREATMENT
0	No Alligator Cracking	20	No Maintenance	No Maintenance
1	Low, Low	16	Routine	Slurry/Chip Seal
2	Low, Medium	10	Rehabilitation	Thin Hot Mix Overlay (<2 in)
3	Low, High	6	Rehabilitation	Thin Hot Mix Overlay (<2 in)
4	Medium, Low	12	Rehabilitation	Thin Hot Mix Overlay (<2 in)
5	Medium, Medium	8	Reconstruct	Thick Overlay (3 in)
6	Medium, High	4	Reconstruct	Rotomill & Thick Overlay
7	High, Low	10	Reconstruct	Thick Overlay (3 in)
8	High, Medium	6	Reconstruct	Cold Recycle & Overlay (3 in)
9	High, High	0	Reconstruct	Full Depth Reclamation (3/3 in.)

Distress deterioration tables with their preservation strategies and recommended treatments similar to those shown in Table 8 were developed for each distress type and are given in Appendix D.

The preservation strategies and recommended treatments given in Appendix F are grouped in the general preservation strategies of routine maintenance, preventative maintenance, rehabilitation, and reconstruction. Each major preservation strategy represents a particular level of work effort and a specific goal with regard to preserving or restoring the pavement.

Routine maintenance is primarily proactive and includes the work items of crack sealing, fog sealing, dig-outs, and patching.

Preventative maintenance is designed to slow pavement deterioration, as well as preserve and improve the functional condition of the pavement. Preventative maintenance strategies do not substantially increase structural capacity. Treatments in the category of preventative maintenance include: sand seals, fog seals, chip seals, scrub seals, cape seals, slurry seals, and microsurfacing.

Rehabilitation serves to correct or remove root causes of distress and to add structural capacity and service life to the pavement. Rehabilitation treatments include thin hot mix asphalt overlays, hot surface recycling, bonded wearing courses, and combinations of leveling courses or rotomilling with overlays.

Reconstruction covers all types of work involved in totally reconstructing or replacing the pavement structure, thus providing a completely new pavement.

A detailed listing of all preservation strategies and their associated treatments with unit costs are given in Appendix F. The unit costs are those provided by Road Science, L.L.C. and are based on the average costs per square yard. A special inventory form built within the Transportation Asset Management System (TAMS) computer program facilitates the analysis process and allows engineering judgment to be exercised at any point. An example of this form is shown in Figure 9. The program uses the previously entered distress information to determine appropriate treatments. For the segment shown in Figure 9, the recommended treatment is a Rotomill & Overlay (< 2 in).

Edit Inventory Information

File Menu

Segment Number

Road Name

From Address

To Address

Number of Travel Lanes

Road Width ft

Segment Length ft

Speed Limit

Surface Type

Owner

Importance

Functional Classification

District

Drainage Type

AADT

Date Inventoried

Photo #

Distress Rating Sheet

Fatigue (0-9)

Longitudinal (0-9)

Transverse (0-9)

Block (0-9)

Patching/Potholes (0-9)

Edge (0-9)

Rutting (0-3)

Roughness (0-3)

Drainage (0-3)

Inventory Date

RSL

Optimal Treatment

☐ RSL based on Date

Suggested Treatment

Figure 9. TAMS Inventory Form

On the left side of the form, inventory information pertaining to the street segment is shown. This information includes the address and location of the segment, surface type, number of lanes, length, width, area, posted speed limit, and date inventoried. On the right side, the various distress ratings are listed, along with a recommended preservation treatment. The “View Picture” button allows the user to look at a digitized photograph of the street segment. The program provides valuable insight into the distresses affecting street segments and the corresponding pavement condition. The program should not be used indiscriminately in selecting pavement treatments. In order to be the most effective, the program must be combined with good engineering judgment, and project level field inspections to make project level analyses. The program should be considered a tool, which the pavement manager can use to improve their decision-making skills, not replace them.

Appendix E shows the initial recommended pavement preservation strategies to be used on each street segment. Table 9 gives an example of the information contained in Appendix E. This information is sorted by treatment type and street name.

Table 9. Recommended Preservation Treatments for Each Segment (Appendix E)

ID	STREET NAME	FROM	TO	CLASS	TREATMENT	AREA (yd ²)
97	400 E	100 S	80 S	Residential	Base/ Pavement Replacement	1144
122	300 W	200 S	100 S	Residential	Crack Seal	1260
202	600 N	100 W	Main Street	Residential	Thin Hot Mix Overlay (<2in)	978
249	Center Street	100 W	Main Street	Residential	Rotomill & Overlay (<2in)	3713
308	275 S	1175 W	1125 W	Residential	Cold Patch	1241
106	100 East	300 S	200 S	Residential	Routine Maintenance	1273

Assessment of Current Street Maintenance Program Funding

Asphalt Road Network

Maintaining and preserving Parowan's street network at a high service level is vital to the wellbeing of the community. It is helpful for elected official to understand that the cost of construction and pavement preservation has gone up 50% in the last seven years. Since the cities are getting the same revenues or less from the B & C gas tax fund, they can preserve only one-half the roads that they could in 2009 with the same money. This is putting road departments in the position of not being able to stay up with cost effective pavement preservation in the early years of a pavement's life. The only solution is to find other sources of funds or let some of the lower functional class roads go, hoping that low volume roads will last a little longer than the higher volume arterials and collectors.

A systematic and balanced pavement preservation program, providing for routine maintenance, preventative maintenance, rehabilitation, and reconstruction, will enable Parowan to cost effectively maintain the street network. A pavement preservation program recommended for cities and towns is one that maintains an estimated average RSL of 10 years with no more than three percent (3%) of the street network at the terminal serviceability level (i.e. RSL = 0). Parowan's 2014 RSL distribution is shown in Figure 10.

RSL Distribution for Asphalt Network

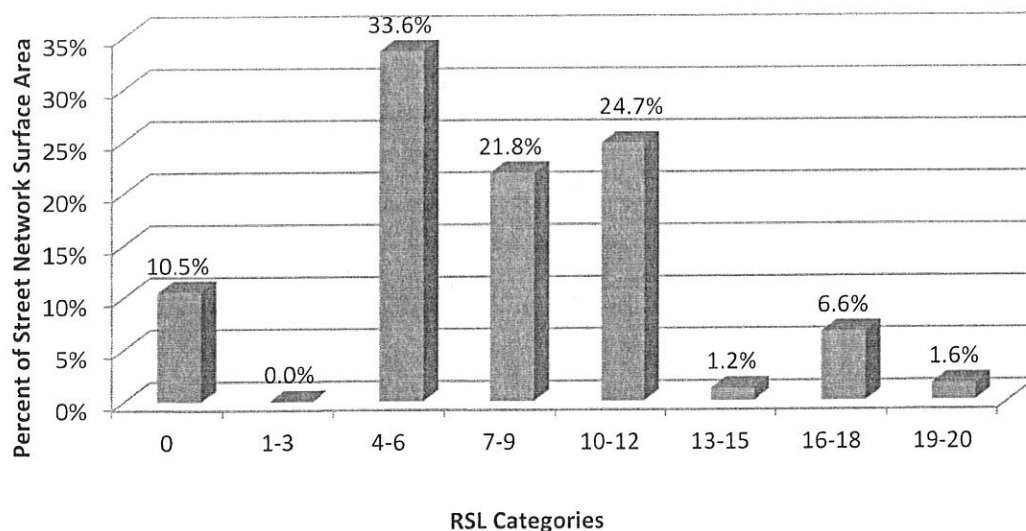


Figure 10. Current RSL Distribution for Asphalt Street Network

The average RSL for Parowan's paved street for 2014 is estimated at 7.8 years with 10.5% of the street network at a terminal service level.

RSL Distribution for Asphalt Network

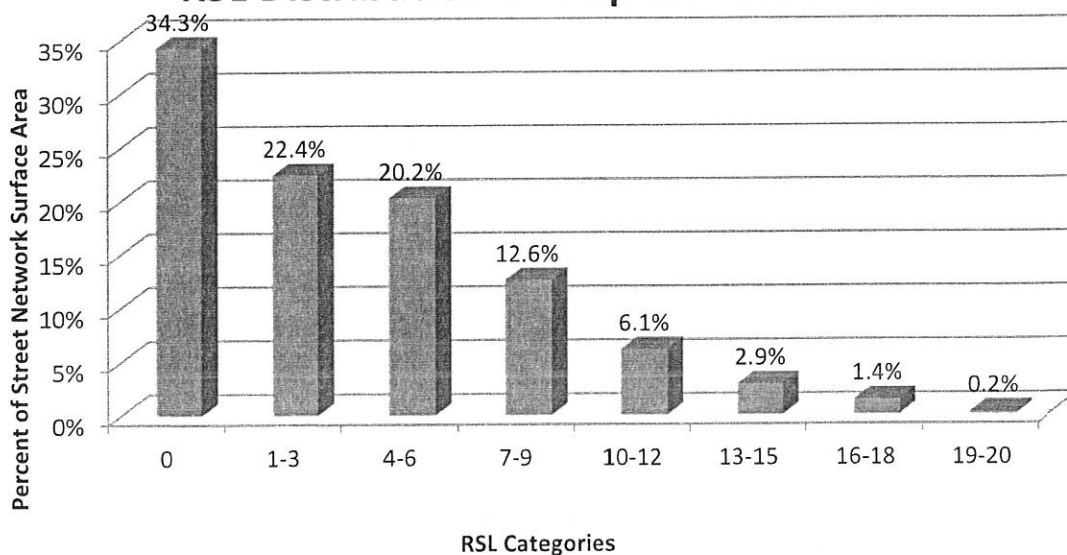


Figure 11 and Figure 12 illustrate the estimated RSL distribution for 2019 and 2024 if no maintenance is performed on the street network. The number of streets at a terminal service level (RSL = 0) would increase from 10.5% to about 67.6% by 2024.

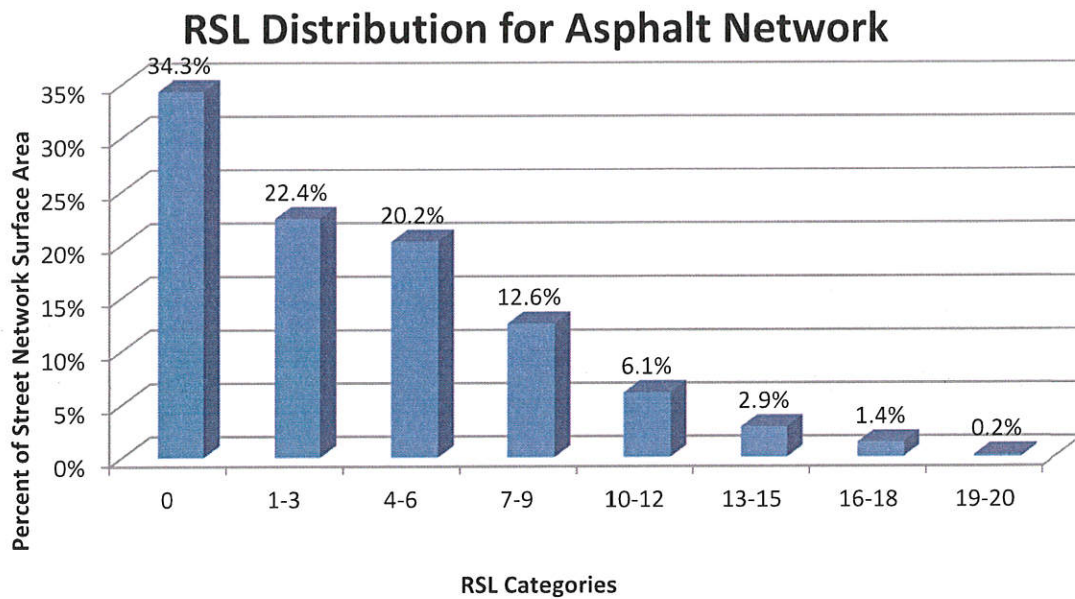


Figure 11. Estimated RSL Distribution for 2019 (if no treatment is done)

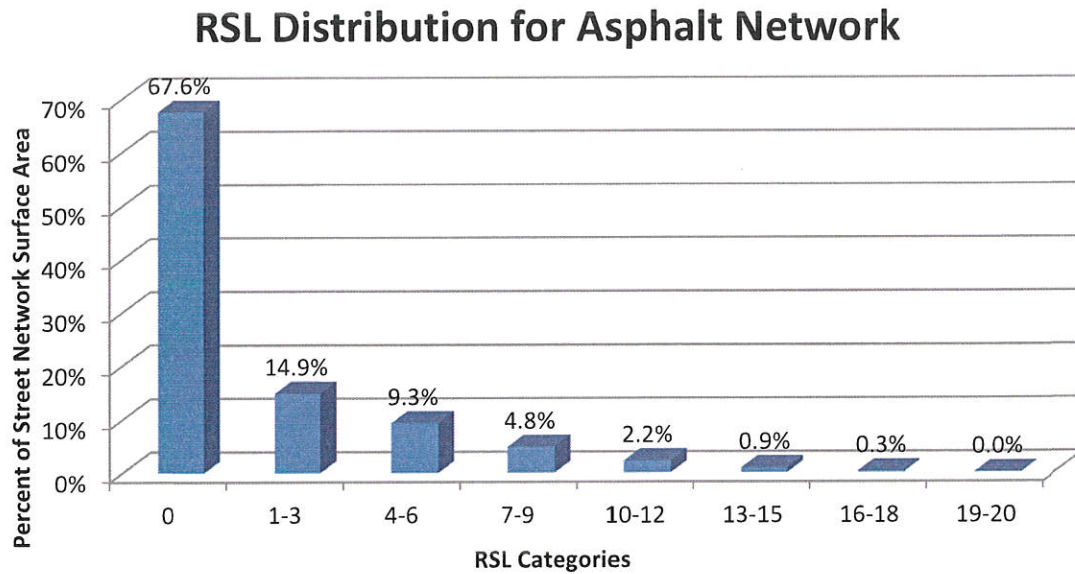


Figure 12. Estimated RSL Distribution for Year 2024 (if no treatment is done)

The resulting estimated average RSL for the year 2019 is 3.8 years and 1.6 years for the year 2024.

Figure 13 and Figure 14 illustrate the estimated RSL distribution for 2019 and 2024 if Parowan City continues on their existing maintenance program funding and allocation of \$94,000 for its streets.

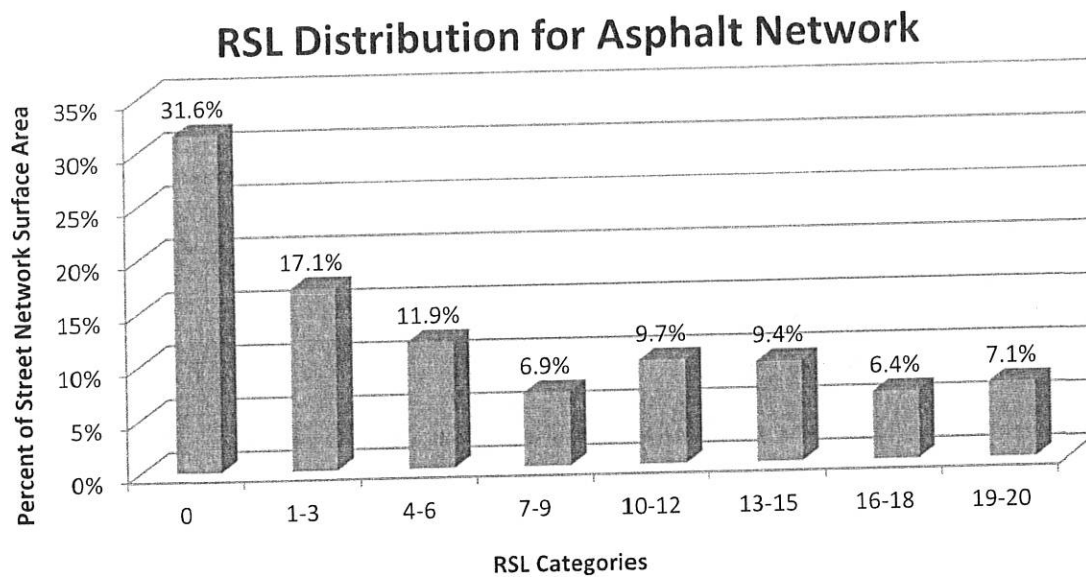


Figure 13. Estimated RSL Distribution for Year 2019 Continuing with Existing Allocation

The number of streets at a terminal service level (RSL = 0) will increase from 10.5% to about 31.6% by 2019. The resulting estimated average RSL for the year 2019 is 6.36 years.

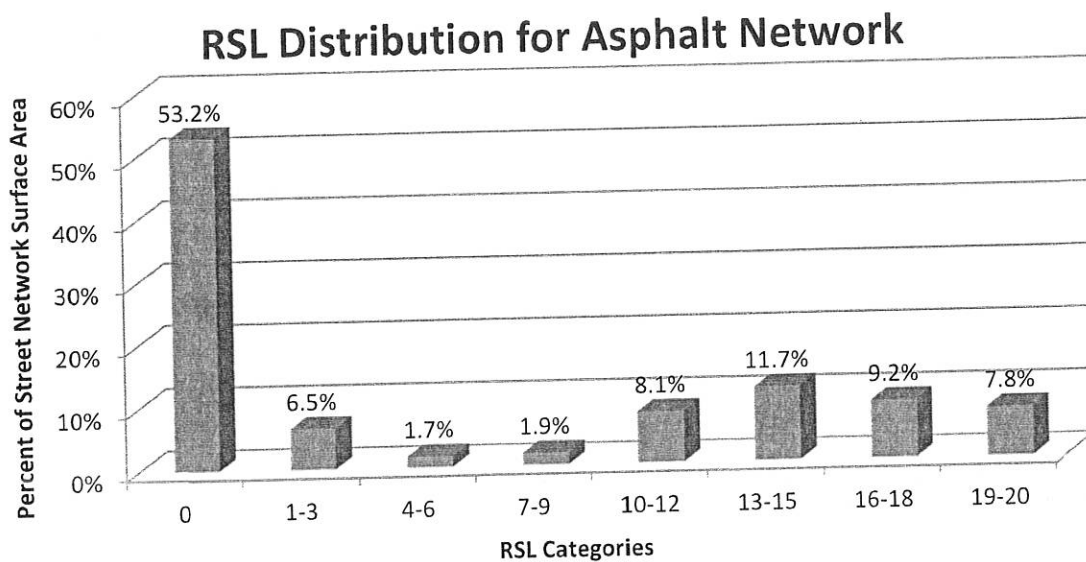


Figure 14. Estimated RSL Distribution for Year 2024 Continuing with Existing Allocation

The number of streets at a terminal service level (RSL = 0) will increase from 10.5% to about 53.2% by 2024. The resulting estimated average RSL for the year 2024 is 6.0 years.

Development of Recommended Pavement Preservation Program

Asphalt Road Network

A two-step pavement preservation program is recommended to increase the level of service of Parowan's road network. Such an approach can increase the estimated average RSL of the road network from 7.8 years to 10.0 years over a 10-year period. The percentage of paved roads at a terminal service level will be 3.0% at the end of this period. This approach achieves the recommended average RSL of at least 10 years of service life with less than 3% of the road network at a terminal service level and maintains the high serviceability level Parowan desires.

The first step in the recommended pavement preservation program deals with the years from 2014 to 2018. A high percentage of preventative maintenance and rehabilitation with some routine maintenance are recommended to decrease the percentage of roads in the "poor" and "fair" categories. Some of the preventative maintenance and rehabilitation strategies are to be applied at less than optimal points in time in order to prevent roads in fair condition from deteriorating too rapidly.

The baseline funding for step one (2014-2018) is estimated to be \$335,200 per year. It is important to note that if a higher amount of money is allocated initially to the asphalt network, it will require less to maintain later. The focus of this step is on roads in the critical RSL range of 4-12 years. Roads in this condition range are at a point where less expensive treatments are effective (i.e. chip seals and thin overlays) but may quickly deteriorate to a point where reconstruction is necessary. The recommended funding distribution for the four pavement preservation strategies is given in Table 10.

Table 10. Paved Road Funding Distribution for 2014-2018 (Step One)

Pavement Preservation Strategies	Percent of Street Network	Funding Distribution
Routine Maintenance	3.0%	\$3,800
Preventive Maintenance	10.0%	\$45,200
Rehabilitation	6.0%	\$286,300
Reconstruction	0.0%	\$0
TOTAL	19.0%	\$335,200

The resulting RSL distribution for in 2019 is shown in Figure 15.

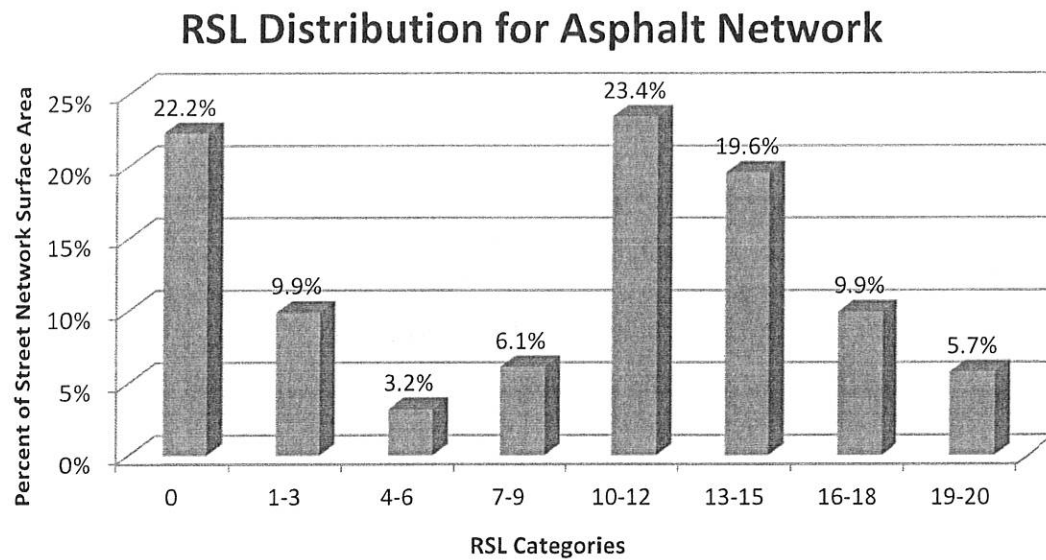


Figure 15. RSL Distribution for 2019 Using Recommended Preservation Program

Figure 15 shows a decreased percentage of roads in fair and poor condition, an improved RSL distribution and an increase in the estimated average RSL to 8.9 years, when compared either to the current distribution or the 2019 distribution using the current funding allocation.

Step two of the pavement preservation program deals with the years 2019-2024. The baseline funding for step two is estimated to be \$448,200 per year. With the improved RSL distribution from the previous step, this phase focuses on reconstructing roads in terminal serviceability while doing enough other preservation work to maintain a good condition distribution. The recommended funding distribution for the four pavement preservation strategies of step two is given in Table 11.

Table 11. Paved Road Funding Distribution for 2019-2024 (Step Two)

Pavement Preservation Strategies	Percent of Street Network	Funding Distribution
Routine Maintenance	2.5%	\$3,100
Preventive Maintenance	4.0%	\$18,100
Rehabilitation	1.0%	\$47,700
Reconstruction	5.3%	\$379,300
TOTAL	12.8%	\$448,200

The resulting RSL distribution for in 2024 is shown in Figure 16.

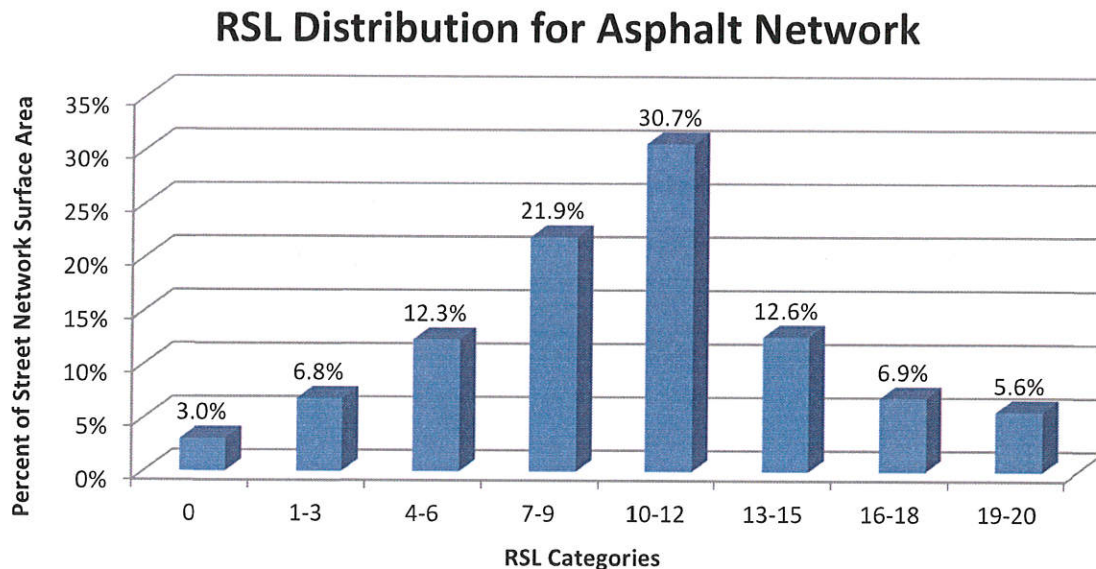


Figure 16. RSL Distribution for 2020 Using Recommended Preservation Program

Figure 16 shows an improved RSL distribution, significantly few roads in failed condition and an increase in the estimated average RSL to 10.0 years. This improved RSL distribution allows the road network to be maintained by strategies that are more cost effective.

The recommended two-step pavement preservation program uses strategies and treatments that are applied at points in time that are the most cost effective. The following tables show the recommended strategies and treatments that are to be applied in each step. In the first step of the pavement preservation program, the percent of road network area receiving treatment is distributed in the RSL categories as shown in Table 12.

Table 12. Paved Road Treatment Distribution for Step One (2014-2019)

Pavement Preservation Strategies	0	1-3	4-6	7-9	10-12	13-15	16-18	19-21	Total
Routine Maintenance						0.8%	2.3%		3.0%
Preventive Maintenance				6.0%	4.0%				10.0%
Rehabilitation			4.5%	1.5%					6.0%
Reconstruction									0.0%

Routine maintenance strategies are recommended to treat three percent (3%) of the asphalt road network with crack seal. Other routine maintenance such as dig out and hot patch can be supplemented in as needed. Preventative maintenance strategies (chip seals) are recommended to treat ten percent (10.0%) of the road network. Rehabilitation maintenance strategies are recommended to treat six percent (6.0%) of the road network with thin hot mix overlay (<2 in.).

The percent of the road network area receiving treatment in the second step of the pavement preservation program is distributed in the RSL categories as shown in Table 13.

Table 13. Paved Road Treatment Distribution for Step Two (2019-2024)

Pavement Preservation Strategies	0	1-3	4-6	7-9	10-12	13-15	16-18	19-21	Total
Routine Maintenance						0.6%	1.9%		2.5%
Preventive Maintenance				2.4%	1.6%				4.0%
Rehabilitation			0.8%	0.3%					1.0%
Reconstruction	5.3%								5.3%

Routine maintenance strategies are recommended to treat three percent (2.5%) of the asphalt road network in step two with crack seal. Other routine maintenance such as dig out and hot patch can be supplemented in as needed. Preventative maintenance strategies (chip seals) are recommended to treat four percent (4.0%) of the road network. Rehabilitation maintenance strategies are recommended to treat 1 percent (1.0%) of the road network with thin hot mix overlay (<2 in.). For roads requiring reconstruction, thick overlay (3 in.) is recommended to treat five percent (5.3%) of the road network.

The suggested baseline budgets for Step One (\$335,200) and Step Two (\$448,200) were calculated using treatment costs for 2014. Table 14 shows the projected budgets for the years 2015-2026 considering a four percent inflation rate per year based on the baseline budgets.

Table 14. Proposed Budget Considering Inflation

STEP ONE (\$335,221 baseline)		STEP TWO (\$448,216 baseline)	
Year	Proposed Budget	Year	Proposed Budget
2014	\$335,221	2019	\$545,323
2015	\$348,630	2020	\$567,136
2016	\$362,575	2021	\$589,822
2017	\$377,078	2022	\$613,415
2018	\$392,161	2023	\$637,951
		2024	\$663,469

Asphalt Road Routine Maintenance

Table 15 shows the roads recommended for routine maintenance in 2014. Segments were selected based on of their level of functional importance to the road network. The treatment recommended for routine maintenance is crack sealing. The total area represented by these segments is approximately 3.0 % of the paved road network.

Table 15. 2014 Routine Maintenance Recommendations

Segment ID	Street	From	To	Area (yd ²)	Treatment
374	Old HWY 91	1810 W	1600 W	4623	Routine Maintenance
373	Old HWY 91	CITY LIMIT	1810 W	8064	Routine Maintenance
282	Old HWY 91	300 S	Main St.	1020	Routine Maintenance

Asphalt Road Preventative Maintenance

Table 16 shows the roads recommended for preventative maintenance in 2014. Segments were selected based on of their level of functional importance to the road network. The treatment recommended for preventative maintenance is chip sealing. The total area represented by these segments is approximately 10.0 % of the paved road network.

Table 16. 2014 Preventative Maintenance Recommendations

Segment ID	Street	From	To	Area (yd ²)	Treatment
19	Old HWY 91	1150 W	1050 W	1901	Preventative Maintenance
302	Old HWY 91	1200 W	1150 W	1192	Preventative Maintenance
304	Old HWY 91	1600 W	1525 W	1724	Preventative Maintenance
303	Old HWY 91	1525 W	1200 W	6851	Preventative Maintenance
28	Old HWY 91	850 W	800 W	933	Preventative Maintenance
138	100 W	100 S	Center St.	5048	Preventative Maintenance
245	1175 W	DEAD END	275 S	681	Preventative Maintenance
171	200 E	200 N	300 N	1260	Preventative Maintenance
187	200 E	500 N	600 N	1224	Preventative Maintenance
326	200 E	600 N	US HWY 271	1011	Preventative Maintenance
62	200 N	400 W	300 W	1512	Preventative Maintenance
63	200 N	500 W	400 W	1491	Preventative Maintenance

Asphalt Road Rehabilitation

Table 17 shows the roads recommended for rehabilitation maintenance in 2014. The treatment recommended for rehabilitation maintenance is a Thin Hot Mix Overlay (<2in). The total area represented by these segments is approximately 6.0% of the paved road network. Roads were selected based on their importance to the road network.

Table 17. 2014 Rehabilitative Maintenance Recommendations

Segment ID	Street	From	To	Area (yd ²)	Treatment
20	Old HWY 91	1050 W	850 W	4303	Rehabilitative Maintenance
266	Old HWY 91	750W	600 W	3077	Rehabilitative Maintenance
27	Old HWY 91	800 W	750 W	1949	Rehabilitative Maintenance
188	Old HWY 91	300 W	200 W	2097	Rehabilitative Maintenance
189	Old HWY 91	200 W	100 W	1814	Rehabilitative Maintenance
376	WEST FRONTAGE ROAD	CITY BOUNDARY	CITY BOUNDARY	8847	Rehabilitative Maintenance
152	100 N	MAIN STREET	100 E	4389	Rehabilitative Maintenance
146	100 W	50 N	100 N	747	Rehabilitative Maintenance
311	1025 W	375 S	DEAD END	1283	Rehabilitative Maintenance
246	1125 W	DEAD END	275 S	712	Rehabilitative Maintenance
114	125 S	CANYON RD	400 E	856	Rehabilitative Maintenance

Asphalt Road Reconstruction

Table 18 prioritizes the roads recommended for reconstructive maintenance. Since no reconstruction is recommended until 2019, this list should be implemented then. The treatment recommended for reconstruction maintenance is thick overlay. The total area represented by these segments is approximately 5.3% of the paved road network. Roads were selected based on their importance to the road network.

Table 18. Reconstruction Maintenance Recommendations

Segment ID	Street	From	To	Area (yd ²)	Treatment
2	Old HWY 91	600 W	300 W	8475	Reconstruction
319	700 N	MAIN STREET	AIRPORT ROAD	400	Reconstruction
158	100 N	400 W	300 W	1258	Reconstruction
313	100 S	400 E	DEAD END	1334	Reconstruction
141	100 W	Center St.	50 N	1272	Reconstruction
199	100 W	600 N	700 N	912	Reconstruction
286	125 S	400 E	DEAD END	1447	Reconstruction
47	150 E	DEAD END	500 N	828	Reconstruction
94	200 W	300 N	400 N	1318	Reconstruction
39	300 S	300 E	CANYON RD	2500	Reconstruction
100	300 S	500 W	400 W	1191	Reconstruction
101	300 S	300 W	200 W	1120	Reconstruction
132	40 S	400 E	500 E	1804	Reconstruction
64	400 E	80 S	40 S	1104	Reconstruction
97	400 E	100 S	80 S	1144	Reconstruction

Implementation of Pavement Management System

A fully implemented pavement management system can be a useful tool to a city, town, or county in cost effectively maintaining their street or road networks at a high service level. Parowan leaders should be applauded for recognizing the value of their street network as well as for their desire to maintain its value and service life.

A majority of the work necessary to implement a pavement management system has been done by the Utah LTAP Center. As described in this report, a full inventory and condition survey of Parowan's street network has been made. This provided the basis for the analyses of the street network's current conditions. In addition, a pavement preservation program and recommendations have been made that will enable Parowan to maintain and enhance the service life of its street network. The Utah LTAP Center has also provided a pavement management computer program that will enable personnel at Parowan to keep accurate and up-to-date records of their street network's conditions and the preservation work that is done.

The following steps are suggested to facilitate the implementation of the pavement management system and assure its beneficial use:

1. Conduct briefings with appropriate personnel to explain the details and procedures of the pavement management system.
2. Install the computer program on the computers of the personnel who are responsible for maintenance of the street network.
3. Train the appropriate personnel how to implement the recommended pavement preservation program.
4. Develop a pavement structure history database including dates of initial construction and subsequent maintenance and rehabilitation actions.
5. Develop a traffic database and incorporate traffic counts, classifications, and axle load data.
6. In cooperation with the personnel responsible for the maintenance of the street network, conduct site reviews of street segments recommended for treatment.
7. Fine-tune the pavement management computer program and establish periodic condition survey and feedback mechanisms to keep the street network conditions current.

The Utah LTAP Center is available and can assist in this implementation effort. Further fieldwork and support is available on an as needed actual cost basis. This can be arranged and scheduled by contacting Nick Jones at the Utah LTAP Center.

Importance of Feedback

The pavement management system set forth in this report is systematic in nature. Therefore, special steps and efforts should be taken to assure that everyone involved has an opportunity and a means to provide both input and feedback in the pavement management process. As shown in Figure 1 **Error! Reference source not found.** in the introduction to this report, feedback among all elements of the pavement management process is essential for the system to be dynamic and useful to the city. Effective feedback has been accomplished by several agencies by establishing a pavement management team or group. This team is comprised of representatives from each operating element involved in the process within the organization. Typically, this team is led by someone from the Public Works Department who assigns specific duties to each team member commensurate with their role in the pavement management process.

The pavement preservation program requires accurate and timely feedback on all decisions and actions taken with respect to preservation (routine maintenance, preventative maintenance, rehabilitation maintenance, and reconstruction) of each street segment. This feedback should include such information as type of work performed, unit costs of work items, amount and quality of work performed, date of completed work, additional pavement structure added, and any other design related information. In addition, periodic condition surveys should be made to keep track of the condition of each street and the network as a whole. These periodic condition surveys should be conducted every two to three years. This feedback information will enable the pavement management team to fine-tune the pavement management computer program providing better information to the decision-makers at all levels.

Any pavement management system must have a means of keeping accurate, up-to-date information about the condition and inventory of the street network. Good decisions are difficult to make without such information. The Transportation Asset Management System (TAMS) computer program provided by the Utah LTAP Center makes this process easy for users. This computer program allows for the inventory of current distress information, tracking of treatments applied, history of work done, and cross section information via pictures of the street segment. These tools provide valuable information that can assist in better decision-making regarding the allocation of resources to maintain and preserve the street network. Figure 17 shows the forms used for the inventorying and updating of the street network.

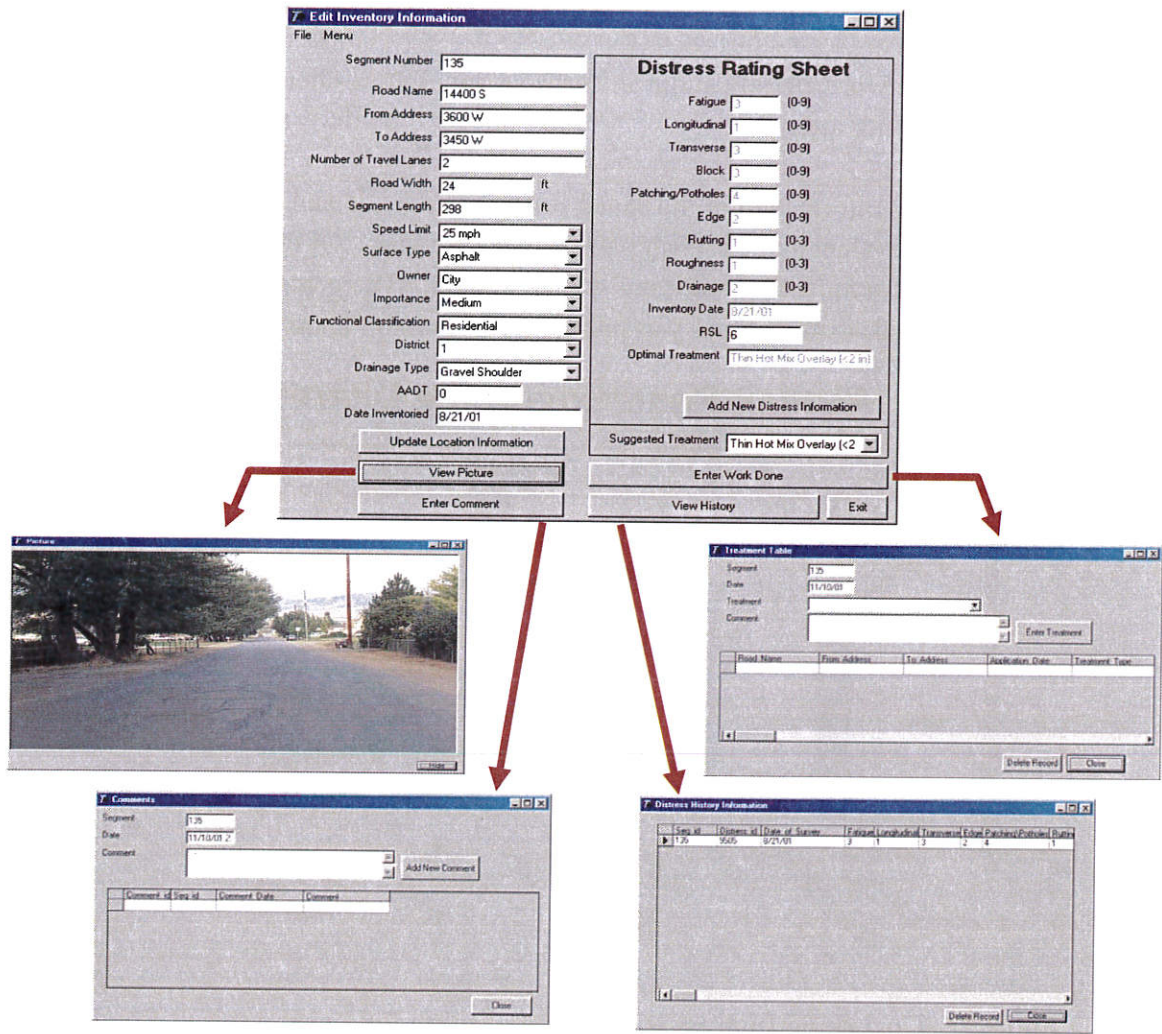


Figure 17. TAMS Inventory and Updating Forms

For documentation on the TAMS program, refer to the TAMS User’s Manual.

Summary of Findings and Recommendations

Findings

Currently the streets network classifications in Parowan are: 92% residential, 2% as minor collector, and 6% as minor arterial.

Analyses of the distress information of the paved street network showed that there were seven major distress types prevalent in the asphalt paved streets network. Of these distress types, edge cracking occurred most frequently in the total streets network. The percent area of the street network affected by these distress types was previously shown in Figure 7.

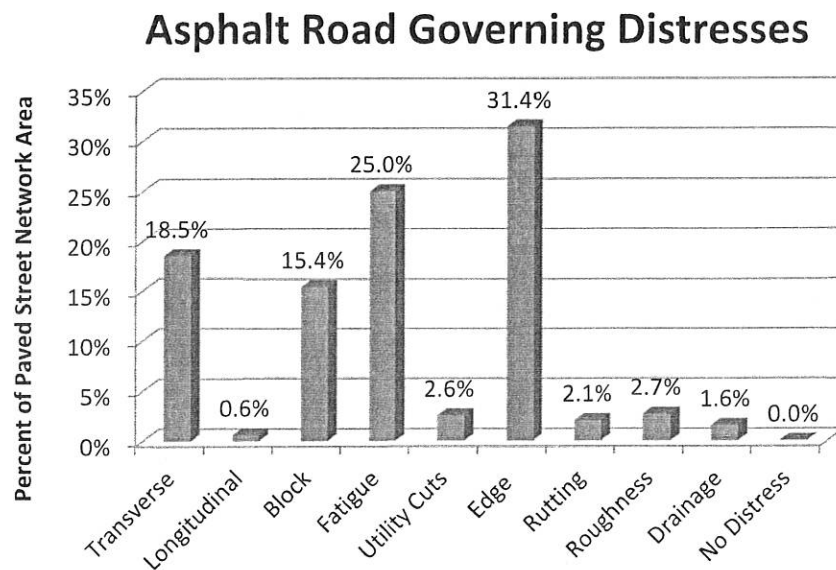


Figure 7. Governing Distress Rating Distribution for Local Asphalt Roads

Currently, the average remaining service life (RSL) for Parowan's entire asphalt paved street network is estimated to be 7.8 years. The percent of street network surface area with no service life left (terminal serviceability or RSL = 0) is 10.5%.

Recommendations

The pavement preservation program recommended for cities and towns is one that maintains the street network at an estimated average remaining service life (RSL) of at least 10 years with no more than three percent (3%) of the street network at the terminal serviceability level. Using the pavement preservation program presented in this report, the estimated average RSL of Parowan's streets network can be increased to approximately 10.0 years by the year 2024. The percent of street network surface area at the terminal serviceability level will be approximately 3.0%. In addition, the RSL distribution of the street network in terms of RSL distribution categories is

improved. With the improved RSL distribution, the most cost-effective strategies and treatments can be used to maintain the street network. The Parowan streets network is currently in a “fair” condition. The public works officials of Parowan should be commended for recognizing the importance of improving and preserving this valuable asset.

A two-step funding level is recommended for preserving the asphalt street networks at a high level of service. The recommended funding for step one is \$335,200 per year and is \$448,200 per year for step two. Costs of expanding the network are not included in this recommended budget. Future funding needs will likely increase due to inflation, increased pavement surface areas, increased traffic volumes, and increased material costs. All road funds should be allocated to pavement preservation. Additional funds required for personnel, capital improvements, and capacity improvements should come from other funding sources such as impact fees and mill levies. The details of this recommended pavement preservation program are given in Appendix H.

City personnel need to be trained in the use of the TAMS computer program so that it may be used and maintained properly. This includes training in data collection and analysis. City employees will be trained and in turn they can train others. Periodic condition surveys, and updates as work is accomplished, are critical in maintaining the pavement management program. Once city personnel are fully trained, the pavement management program can be fine-tuned to fully incorporate the knowledge and experience of Parowan Public Works and Engineering Departments.

It has been a pleasure working with Parowan to provide the information included in this report. Parowan Public Works has been extremely supportive of the work that has been done in preparing the pavement preservation program. The pavement management program can be used to maintain and improve the streets network for several years to come.

Table 19. Summary of Findings and Recommendations

2014 Average RSL	7.8
2014 Terminal Serviceability	10.5%
2024 Estimated Average RSL	10.0
2024 Estimated Terminal Serviceability	3.0%
2014-2024 Average Recommended Annual Funding	\$403,000

Appendix A

Inventory of Street Network

Inventory Report - Asphalt

ID	Road Name	From	To	Class	Width	Length	RSL	Area (yd)
1	1150 W	575 S	Old HWY 91	Residential	26	1257	6	3631
2	Old HWY 91	600 W	300 W	Residential	45	1695	0	8475
3	CENTER STREET	600 W	500 W	Residential	21	562	6	1311
4	600 W	Old HWY 91	400 S	Residential	29	539	16	1737
5	400 S	600 W	500 W	Residential	18	521	6	1042
6	600 W	400 S	300 S	Residential	29	506	8	1630
7	300 S	600 W	500 W	Residential	18	564	4	1128
8	600 W	100 S	CENTER STREET	Residential	32	704	10	2503
9	600 W	100 N	200 N	Residential	32	629	10	2236
10	200 N	600 W	500 W	Residential	24	611	8	1629
11	750 W	130 N	200 N	Residential	35	471	6	1832
12	200 N	750 W	600 W	Residential	23	709	8	1812
13	780 W	130 N	200 N	Residential	35	471	0	1832
14	200 N	780 W	750 W	Residential	30	277	8	923
15	200 N	825 W	780 W	Residential	30	308	8	1027
16	850 W	DEAD END	200 N	Residential	25	658	6	1828
17	200 N	850 W	825 W	Residential	23	334	8	854
18	500 W	400 S	300 S	Residential	18	497	8	994
19	Old HWY 91	1150 W	1050 W	Minor Arterial	28	611	8	1901

ID	Road Name	From	To	Class	Width	Length	RSL	Area (yd)
20	Old HWY 91	1050 W	850 W	Minor Arterial	28	1383	6	4303
21	1200 W	275 S	300 S	Residential	33	268	10	983
22	100 S	600 W	500 W	Residential	18	559	20	1118
23	100 N	600 W	500 W	Residential	20	559	6	1242
24	130 N	780 W	750 W	Residential	35	280	4	1089
25	200 N	875 W	850 W	Residential	20	331	8	736
26	200 N	900 W	875 W	Residential	20	328	8	729
27	Old HWY 91	800 W	750 W	Residential	53	331	6	1949
28	Old HWY 91	850 W	800 W	Minor Arterial	27	311	12	933
31	300 N	400 W	300 W	Residential	21	565	8	1318
32	300 E	400 N	500 N	Residential	25	558	6	1550
34	300 S	200 W	100 W	Residential	18	601	6	1202
36	300 S	MAIN STREET	80 E	Residential	18	433	8	866
37	300 S	100 W	Old HWY 91	Residential	15	659	8	1098
39	300 S	300 E	CANYON RD	Residential	37	608	0	2500
40	300 E	300 S	200 S	Residential	34	468	16	1768
42	400 W	400 N	DEAD END	Residential	19	349	4	737
43	500 N	250 E	300 E	Residential	24	289	4	771
44	200 E	400 N	500 N	Residential	20	560	6	1244
45	500 N	200 E	250 E	Residential	24	274	6	731

ID	Road Name	From	To	Class	Width	Length	RSL	Area (yd)
46	100 E	400 N	500 N	Residential	21	561	10	1309
47	150 E	DEAD END	500 N	Residential	36	207	0	828
48	500 N	150 E	200 E	Residential	24	275	0	733
50	100 W	500 N	600 N	Residential	21	561	6	1309
52	100 E	500 N	US HWY 271	Residential	21	601	16	1402
53	350 W	500 N	DIANE DRIVE	Residential	36	684	4	2736
54	MAIN STREET	300 S	OLD HWY 91	Residential	19	308	8	650
55	225 S	CANYON DRIVE	465 E	Residential	26	252	4	728
56	465 E	CITY VIEW DRIVE	200 S	Residential	36	138	0	552
57	CENTER STREET	200 W	100 W	Residential	23	601	6	1536
58	400 W	100 S	CENTER STREET	Residential	21	699	4	1631
59	CENTER STREET	400 W	300 W	Residential	23	561	6	1434
60	CENTER STREET	500 W	400 W	Residential	21	560	10	1307
61	400 W	100 N	200 N	Residential	22	570	4	1393
62	200 N	400 W	300 W	Residential	24	567	8	1512
63	200 N	500 W	400 W	Residential	24	559	8	1491
64	400 E	80 S	40 S	Residential	36	276	0	1104
65	300 E	200 N	300 N	Residential	25	572	8	1589
66	300 N	300 E	DEAD END	Residential	50	317	4	1761
68	100 E	200 N	300 N	Residential	21	565	16	1318

ID	Road Name	From	To	Class	Width	Length	RSL	Area (yd)
69	300 N	100 E	200 E	Residential	21	567	8	1323
70	300 N	MAIN STREET 100 W	100 E	Residential	60	634	8	4227
71	300 N	100 W	MAIN STREET 300 N	Residential	40	691	12	3071
72	200 W	200 N	300 N	Residential	21	565	6	1318
73	300 N	200 W	100 W	Residential	40	589	6	2618
74	300 W	200 N	300 N	Residential	23	562	8	1436
75	300 N	300 W	200 W	Residential	21	562	8	1311
76	300 S	80 E	100 E	Residential	18	138	8	276
77	400 W	DEAD END	300 S	Residential	13	327	4	472
78	300 S	100 E	200 E	Residential	20	592	12	1316
80	300 W	Old HWY 91	300 S	Residential	23	617	8	1577
82	300 S	400 W	300 W	Residential	19	560	8	1182
83	200 E	300 N	400 N	Residential	20	568	10	1262
84	100 E	300 N	400 N	Residential	21	565	16	1318
85	400 N	100 E	200 E	Residential	22	570	4	1393
87	400 N	200 E	300 E	Residential	22	567	8	1386
88	400 N	MAIN STREET 300 N	100 E	Residential	63	634	4	4438
89	100 W	300 N	400 N	Residential	21	567	6	1323
90	400 N	100 W	MAIN STREET 400 N	Residential	28	689	8	2144
91	300 W	300 N	400 N	Residential	23	563	10	1439

ID	Road Name	From	To	Class	Width	Length	RSL	Area (yd)
92	400 W	300 N	400 N	Residential	22	559	6	1366
93	400 N	400 W	300 W	Residential	28	565	8	1758
94	200 W	300 N	400 N	Residential	21	565	6	1318
95	400 N	200 W	100 W	Residential	28	585	6	1820
96	400 N	300 W	200 W	Residential	28	567	6	1764
97	400 E	100 S	80 S	Residential	36	286	0	1144
98	80 S	400 E	500 E	Residential	36	353	0	1412
99	400 W	200 N	300 N	Residential	22	562	6	1374
100	300 S	500 W	400 W	Residential	19	564	6	1191
101	300 S	300 W	200 W	Residential	18	560	6	1120
102	Old HWY 91	100 W	300 S	Residential	30	752	6	2507
103	100 W	300 S	200 S	Residential	18	480	8	960
104	200 W	300 S	200 S	Residential	22	488	16	1193
106	100 E	300 S	200 S	Residential	23	498	16	1273
107	200 S	100 E	200 E	Residential	30	577	10	1923
108	200 S	MAIN STREET	100 E	Residential	30	626	8	2087
109	400 W	300 S	200 S	Residential	17	494	4	933
110	300 W	300 S	200 S	Residential	23	492	10	1257
114	125 S	CANYON RD	400 E	Residential	35	220	4	856
115	300 E	200 S	100 S	Residential	24	491	6	1309

ID	Road Name	From	To	Class	Width	Length	RSL	Area (yd)
116	200 E	200 S	100 S	Residential	20	497	6	1104
117	100 S	200 E	300 E	Residential	20	563	20	1251
118	100 E	200 S	100 S	Residential	23	493	16	1260
119	100 S	100 E	200 E	Residential	20	572	16	1271
121	100 S	MAIN STREET	100 E	Residential	30	627	16	2090
122	300 W	200 S	100 S	Residential	23	493	10	1260
123	100 S	300 W	200 W	Residential	18	564	16	1128
124	200 W	200 S	100 S	Residential	21	498	16	1162
125	400 W	200 S	100 S	Residential	21	498	4	1162
126	100 S	400 W	300 W	Residential	18	560	16	1120
127	100 W	200 S	100 S	Residential	23	502	8	1283
128	100 S	500 W	400 W	Residential	18	562	16	1124
129	100 S	200 W	100 W	Residential	18	600	16	1200
130	400 E	125 S	100 S	Residential	35	263	0	1023
131	500 E	80 S	40 S	Residential	36	268	0	1072
132	40 S	400 E	500 E	Residential	46	353	0	1804
133	300 E	100 S	Center St.	Residential	40	655	6	2911
134	200 E	100 S	Center St.	Residential	20	702	6	1560
135	100 E	100 S	Center St.	Residential	89	699	8	6912
137	300 W	100 S	CENTER STREET	Residential	23	700	8	1789

ID	Road Name	From	To	Class	Width	Length	RSL	Area (yd)
138	100 W	100 S	Center St.	Residential	65	699	8	5048
139	CENTER STREET	300 W	200 W	Residential	23	559	6	1429
140	200 W	100 S	CENTER STREET	Residential	21	700	10	1633
141	100 W	Center St.	50 N	Residential	25	458	6	1272
142	50 N	100 W	MAIN STREET	Residential	33	687	0	2519
143	100 E	Center St.	60 N	Residential	25	480	10	1333
144	60 N	MAIN STREET	100 E	Residential	21	630	4	1470
146	100 W	50 N	100 N	Residential	25	269	4	747
147	100 N	100 W	MAIN STREET	Residential	60	688	16	4587
148	200 E	Center St.	100 N	Residential	20	753	6	1673
149	100 N	200 E	300 E	Residential	24	565	6	1507
150	100 E	60 N	100 N	Residential	25	272	10	756
151	100 N	100 E	200 E	Residential	24	574	6	1531
152	100 N	MAIN STREET	100 E	Residential	63	627	4	4389
153	200 W	CENTER STREET	100 N	Residential	21	753	8	1757
154	100 N	200 W	100 W	Residential	60	594	20	3960
155	300 W	CENTER STREET	100 N	Residential	23	764	8	1952
156	100 N	300 W	200 W	Residential	20	558	6	1240
157	400 W	CENTER STREET	100 N	Residential	22	765	6	1870
158	100 N	400 W	300 W	Residential	20	566	6	1258

ID	Road Name	From	To	Class	Width	Length	RSL	Area (yd)
159	100 N	500 W	400 W	Residential	20	559	6	1242
160	300 E	100 N	200 N	Residential	25	577	8	1603
161	200 N	300 W	OLD PARAGON	Residential	25	824	16	2289
162	200 E	100 N	200 N	Residential	20	567	6	1260
163	200 N	200 E	300 E	Residential	22	566	10	1384
164	100 E	100 N	200 N	Residential	21	572	10	1335
165	200 N	100 E	200 E	Residential	22	573	16	1401
167	200 W	100 N	200 N	Residential	21	571	8	1332
168	200 N	MAIN STREET	100 E	Residential	22	629	16	1538
169	300 W	100 N	200 N	Residential	23	566	10	1446
170	200 N	300 W	200 W	Residential	24	557	8	1485
171	200 E	200 N	300 N	Residential	20	567	8	1260
172	300 N	200 E	300 E	Residential	21	567	8	1323
173	300 N	500 W	400 W	Residential	21	563	8	1314
174	400 N	400 E	DEAD END	Residential	50	270	10	1500
176	400 N	300 E	400 W	Residential	50	558	10	3100
177	400 N	500 W	400 W	Residential	28	567	8	1764
178	200 W	400 N	DEAD END	Residential	21	262	8	611
179	500 N	100 E	150 E	Residential	22	296	4	724
181	500 N	MAIN STREET	100 E	Residential	22	635	4	1552

ID	Road Name	From	To	Class	Width	Length	RSL	Area (yd)
182	100 W	400 N	500 N	Residential	21	563	6	1314
183	500 N	100 W	MAIN STREET	Residential	22	692	6	1692
185	300 E	500 N	600 N	Residential	25	547	10	1519
186	250 E	500 N	600 N	Residential	25	551	10	1531
187	200 E	500 N	600 N	Residential	20	551	8	1224
188	Old HWY 91	300 W	200 W	Residential	28	674	6	2097
189	Old HWY 91	200 W	100 W	Residential	28	583	6	1814
190	500 W	300 N	400 N	Residential	21	555	4	1295
191	500 W	200 N	300 N	Residential	21	568	6	1325
192	500 W	100 N	200 N	Residential	21	569	6	1328
193	500 W	CENTER STREET	100 N	Residential	21	765	6	1785
194	500 W	100 S	CENTER STREET	Residential	21	702	4	1638
195	500 W	200 S	100 S	Residential	21	493	6	1150
196	500 W	300 S	200 S	Residential	22	492	6	1203
198	300 E	600 N	UT HWY 271	Residential	25	386	8	1072
199	100 W	600 N	700 N	Residential	21	391	6	912
201	700 N	100 W	Main St.	Residential	24	708	10	1888
202	600 N	100 W	MAIN STREET	Residential	20	688	8	1529
203	600	200 E	250 E	Residential	24	273	12	728
204	600 N	250 E	300 E	Residential	23	296	10	756

ID	Road Name	From	To	Class	Width	Length	RSL	Area (yd)
205	200 S	200 E	300 E	Residential	30	556	14	1853
212	1000 W	DEAD END	375 S	Residential	35	146	14	568
213	1200 W	DEAD END	375 S	Residential	45	209	12	1045
214	375 S	1375 W	1200 W	Residential	45	1061	12	5305
215	1200 W	375 S	300 S	Residential	45	284	6	1420
216	Parowan Heritage Park	DEAD END	Old HWY 91	Residential	45	628	6	3140
218	1000 W	375 S	275 S	Residential	40	614	10	2729
220	MAIN STREET	DEAD END	300 S	Residential	19	349	10	737
224	60 N	775 W	725 W	Residential	50	376	4	2089
225	725 W	60 N	100 N	Residential	45	261	6	1305
226	775 W	60 N	100 N	Residential	45	269	6	1345
227	100 N	775 W	725 W	Residential	25	373	6	1036
228	200 N	OLD PARAGONA	END OF PAVEMEN	Residential	25	1123	10	3119
229	560 W	200 N	400 N	Residential	23	1467	8	3749
229	560 W	200 N	400 N	Residential	23	1467	8	3749
231	500 N	350 W	DEAD END	Residential	35	159	20	618
232	500 N	DIANE DRIVE	425 W	Residential	30	374	12	1247
233	500 N	425 W	350 W	Residential	36	297	8	1188
234	50 W	700 N	DEAD END	Residential	43	228	8	1089
241	1075 W	375 S	275 S	Residential	35	577	10	2244

ID	Road Name	From	To	Class	Width	Length	RSL	Area (yd)
242	850 W	DEAD END	Old HWY 91	Residential	25	439	0	1219
243	1125 W	275 S	DEAD END	Residential	35	179	6	696
244	1175 W	275 S	DEAD END	Residential	35	185	10	719
245	1175 W	DEAD END	275 S	Residential	35	175	8	681
246	1125 W	DEAD END	275 S	Residential	35	183	4	712
248	300 E	Center St.	100 N	Residential	25	792	10	2200
249	CENTER STREET	100 W	MAIN STREET	Residential	49	682	6	3713
252	600 W	300 N	DEAD END	Residential	45	280	12	1400
253	325 N	560 W	600 W	Residential	40	655	6	2911
254	900 W	DEAD END	200 N	Residential	19	872	6	1841
255	825 W	DEAD END	200 N	Residential	22	654	8	1599
256	100 N	725 W	DEAD END	Residential	25	228	10	633
257	875 W	DEAD END	200 N	Residential	26	976	0	2820
258	300 N	560 W	600 W	Residential	40	252	10	1120
259	300 N	560 W	500 W	Residential	40	311	10	1382
260	560 W	300 N	DEAD END	Residential	34	180	10	680
262	200 E	300 S	200 S	Residential	20	481	14	1069
263	750 W	END PAVEMENT	Old HWY	Residential	42	1129	6	5269
266	Old HWY 91	750W	600 W	Residential	28	989	6	3077
268	Heritage Hills Drive	END OF PAVEMENT	Pinion Circle	Residential	35	303	12	1178

ID	Road Name	From	To	Class	Width	Length	RSL	Area (yd)
269	Pinion Circle	DEAD END	1525 W	Residential	35	485	10	1886
270	1525 W	Old HWY 91	750 S	Residential	35	552	16	2147
271	725 S	1600 W	DEAD END	Residential	35	387	10	1505
273	Fairground	Loop	Loop	Residential	30	1051	0	3503
274	Fairground	300 E	Loop	Residential	20	1041	0	2313
275	RED HILLS CIRCLE	465 E	DEAD END	Residential	36	179	0	716
281	N. Airport Rd.	700 N	1000 N	Residential	25	1801	0	5003
282	Old HWY 91	300 S	Main St.	Residential	30	306	8	1020
283	200 S	300 E	CANYON RD	Residential	20	445	8	989
286	125 S	400 E	DEAD END	Residential	35	372	0	1447
287	225 SOUTH	465 E	500 E	Residential	23	563	4	1439
290	950 S	1600 W	END OF PAVEMEN	Residential	41	54	20	246
291	1600 W	950 S	725 S	Residential	38	1563	10	6599
292	1600 W	725 S	Old HW Y 91	Residential	37	226	12	929
293	325 S	1400 W	1375 W	Residential	45	340	14	1700
294	1375 W	325 S	275 S	Residential	45	345	10	1725
295	1375 W	375 S	325 S	Residential	45	323	8	1615
296	1375 W	DEAD END	375 S	Residential	45	136	14	680
297	1050 W	575 S	Old HWY 91	Residential	26	1377	10	3978
298	575 S	1150 W	1050 W	Residential	26	617	8	1782

ID	Road Name	From	To	Class	Width	Length	RSL	Area (yd)
299	1200 W	LANDFILL	Old HWY 91	Residential	21	1158	6	2702
300	2200 W	475 S	450 S	Residential	12	217	20	289
302	Old HWY 91	1200 W	1150 W	Minor Arterial	28	383	8	1192
303	Old HWY 91	1525 W	1200 W	Minor Arterial	28	2202	10	6851
304	Old HWY 91	1600 W	1525 W	Minor Arterial	28	554	8	1724
305	275 S	1025 W	1000 W	Residential	35	178	10	692
306	275 S	1075 W	1025 W	Residential	35	312	10	1213
307	275 S	1125 W	1075 W	Residential	35	319	8	1241
308	275 S	1175 W	1125 W	Residential	35	319	10	1241
309	275 S	1200 W	1175 W	Residential	35	185	10	719
310	375 S	1025 W	1000 W	Residential	35	177	8	688
311	1025 W	375 S	DEAD END	Residential	35	330	4	1283
312	375 S	1075 W	1025 W	Residential	35	302	10	1174
313	100 S	400 E	DEAD END	Residential	35	343	0	1334
314	50 W	200 S	100 S	Residential	13	506	10	731
315	100 S	50 W	MAIN STREET	Residential	18	361	16	722
316	100 S	100 W	50 W	Residential	18	323	20	646
319	700 N	MAIN STREET	AIRPORT ROAD	Minor Collector	25	144	0	400
321	475 S	2300 W	2200 W	Residential	31	714	6	2459
323	450 S	2300 W	2200 W	Residential	31	715	6	2463

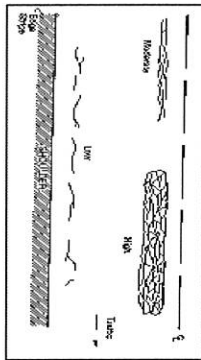
ID	Road Name	From	To	Class	Width	Length	RSL	Area (yd)
325	Fairground	City View Dr.	DEAD END	Residential	29	907	0	2923
326	200 E	600 N	US HWY 271	Residential	20	455	8	1011
333	1075 W	275 S	DEAD END	Residential	35	192	6	747
334	1025 W	275 S	DEAD END	Residential	35	193	10	751
335	375 S	1200 W	DEAD END	Residential	45	533	6	2665
336	275 S	1375 W	DEAD END	Residential	45	445	10	2225
337	325 S	1375 W	DEAD END	Residential	45	368	10	1840
338	600 W	CENTER STREET	100 N	Residential	32	763	8	2713
339	60 N	725 W	600 W	Residential	50	682	4	3789
341	N. Airport Rd.	50 E	END OF PAVEMEN 200 S	Residential	25	1494	0	4150
342	1200 W	275 S	200 S	Residential	33	558	10	2046
344	1375 W	275 S	200 S	Residential	28	486	10	1512
348	1000 W	275 S	200 S	Residential	40	558	10	2480
350	800 W	DEAD END	200 S	Residential	34	505	6	1908
352	750 W	DEAD END	200 S	Residential	34	502	10	1896
353	700 W	DEAD END	200 S	Residential	34	493	0	1862
357	600 W	300 S	200 S	Residential	29	473	12	1524
358	600 W	200 S	100 S	Residential	32	499	10	1774
363	400 N	575 W	500 W	Residential	28	363	8	1129
364	400 N	WEST FRONTAGE	575 W	Residential	31	1396	6	4808

ID	Road Name	From	To	Class	Width	Length	RSL	Area (yd)
368	500 W	400 N	450 N	Residential	22	292	6	714
369	500 N	500 W	DIANE DRIVE	Residential	22	216	12	528
370	400 E	DEAD END	400 N	Residential	50	438	6	2433
373	Old HWY 91	CITY LIMIT	1810 W	Minor Arterial	28	2592	12	8064
374	Old HWY 91	1810 W	1600 W	Minor Arterial	28	1486	10	4623
375	2200 W	500 S	475 S	Residential	12	128	12	171
376	WEST FRONTAGE	CITY BOUNDARY	CITY BOUNDAR	Minor Collector	25	3185	8	8847

Appendix B

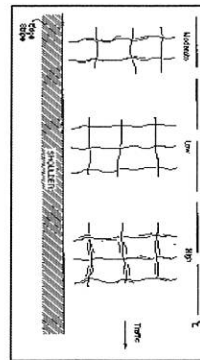
Condition Survey Evaluation Sheet

FATIGUE CRACKING



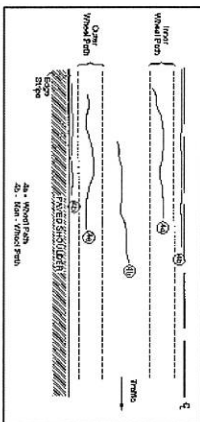
Severity	Extent		
	Low	Medium	High
0 None	1 Crack WP or 1' off C&G Length	2 Crack WP or 1'-2' off C&G Length	>30% of Surface Area or Length
Low Cracks < 1/4"	1	2	3
Medium Cracks 1/4" to 3/4"	4	5	6
High Cracks > 3/4"	7	8	9

BLOCK CRACKING



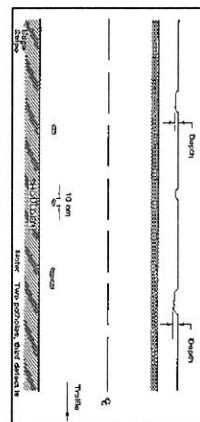
Severity	Extent		
	Low	Medium	High
0 None	> 15'x15' Squares	15'-10'x Squares	< 10'x10' Squares
Low Cracks < 1/4"	1	2	3
Medium Cracks 1/4" to 3/4"	4	5	6
High Cracks > 3/4"	7	8	9

LONGITUDINAL CRACKING



Severity	Extent		
	Low	Medium	High
0 None	1 Crack Full Length	2 Cracks Full Length	> 2 Cracks Full Length
Low Cracks < 1/4"	1	2	3
Medium Cracks 1/4" to 3/4"	4	5	6
High Cracks > 3/4"	7	8	9

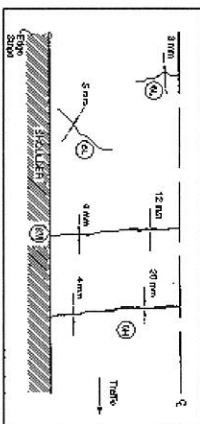
UTILITY CUTS



Severity	Extent		
	Low	Medium	High
0 None	0-10% of Length	10-30% of Length	>30% of Length
Low Cracks < 1/4"	1	2	3
Medium Cracks 1/4" to 3/4"	4	5	6
High Cracks > 3/4"	7	8	9

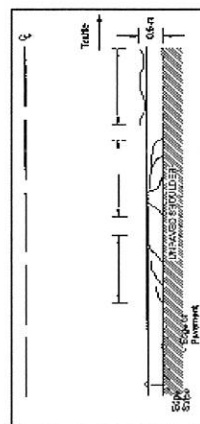
Note: to rate potholes use the same form with the following changes to the severity: Low is <1" deep, Med is 1"-2" deep and High is >2"

TRANSVERSE CRACKING



Severity	Extent		
	Low	Medium	High
0 None	> 100' between Cracks	100'-20' between Cracks	< 20' between Cracks
Low Cracks < 1/4"	1	2	3
Medium Cracks 1/4" to 3/4"	4	5	6
High Cracks > 3/4"	7	8	9

EDGE CRACKING



Severity	Extent		
	Low	Medium	High
0 None	0-10% of Length	10-30% of Length	> 30% of Length
Low 0-6" from Curb	1	2	3
Medium 6-18" from Curb	4	5	6
High 18" from Curb	7	8	9

Drainage / Roughness

Excellent	Good	Fair	Poor
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Rutting

Excellent 0	Low <3/8"	Med 1/2"-3/4"	High >3/4"
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Appendix C

Condition Survey of Street Network

Condition Report - Asphalt

ID	Road Name	From	To	RSL	Tra	Long	Block	Fatigu	Patch	Edge	Rutting	Rough	Drain	Survey Date
1	1150 W	575 S	Old HWY	6	8	0	0	5	7	0	0	0	0	11/14/2014
2	Old HWY 91	600 W	300 W	0	0	0	0	9	1	0	0	1	0	11/14/2014
3	CENTER STREET	600 W	500 W	6	0	0	0	0	0	6	0	2	3	11/14/2014
4	600 W	Old HWY 91	400 S	16	1	0	0	1	1	0	0	0	0	11/14/2014
5	400 S	600 W	500 W	6	5	0	0	1	0	6	0	1	0	11/14/2014
6	600 W	400 S	300 S	8	0	1	0	0	0	5	0	0	0	11/14/2014
7	300 S	600 W	500 W	4	0	0	0	0	7	9	0	2	0	11/14/2014
8	600 W	100 S	CENTER R	10	5	0	0	2	0	0	0	1	3	11/15/2014
9	600 W	100 N	200 N	10	4	0	0	0	0	4	0	1	0	11/15/2014
10	200 N	600 W	500 W	8	0	0	0	4	0	3	0	1	0	11/14/2014
11	750 W	130 N	200 N	6	0	0	5	8	1	0	0	0	3	11/14/2014
12	200 N	750 W	600 W	8	0	0	0	0	0	3	0	1	3	11/14/2014

ID	Road Name	From	To	RSL	Tra	Long	Block	Fatigu	Patch	Edge	Rutting	Rough	Drain	Survey Date
13	780 W	130 N	200 N	0	0	0	0	9	0	0	1	2	3	11/14/2014
14	200 N	780 W	750 W	8	0	0	0	0	4	3	0	1	3	11/14/2014
15	200 N	825 W	780 W	8	0	0	0	0	0	3	0	1	0	11/14/2014
16	850 W	DEAD END	200 N	6	0	0	0	5	0	6	0	0	3	11/14/2014
17	200 N	850 W	825 W	8	0	0	0	0	1	3	0	0	2	11/14/2014
18	500 W	400 S	300 S	8	2	0	0	0	8	5	0	0	0	11/14/2014
19	Old HWY 91	1150 W	1050 W	8	0	0	5	0	0	0	0	0	0	11/14/2014
20	Old HWY 91	1050 W	850 W	6	0	0	6	0	1	0	0	0	0	11/14/2014
21	1200 W	275 S	300 S	10	5	1	0	0	0	0	0	0	2	11/14/2014
22	100 S	600 W	500 W	20	0	0	0	0	0	0	0	0	1	11/14/2014
23	100 N	600 W	500 W	6	0	0	6	0	0	5	0	2	0	11/14/2014
24	130 N	780 W	750 W	4	0	0	0	6	3	0	0	2	0	11/14/2014
25	200 N	875 W	850 W	8	0	0	0	1	0	3	0	0	1	11/14/2014

ID	Road Name	From	To	RSL	Tra	Long	Block	Fatigu	Patch	Edge	Rutting	Rough	Drain	Survey Date
26	200 N	900 W	875 W	8	0	0	0	4	0	3	0	2	0	11/14/2014
27	Old HWY 91	800 W	750 W	6	0	0	8	0	0	0	0	0	1	11/14/2014
28	Old HWY 91	850 W	800 W	12	0	0	7	0	0	0	0	0	0	11/14/2014
31	300 N	400 W	300 W	8	0	0	5	0	0	3	0	0	3	11/14/2014
32	300 E	400 N	500 N	6	0	4	0	8	0	0	0	0	0	11/15/2014
34	300 S	200 W	100 W	6	3	0	0	4	0	6	0	0	0	11/14/2014
36	300 S	MAIN STREET	80 E	8	0	0	0	5	0	3	0	1	1	11/14/2014
37	300 S	100 W	Old HWY	8	2	0	0	0	4	5	0	0	0	11/14/2014
39	300 S	300 E	CANYO N RD	0	0	0	0	9	0	0	0	0	0	11/14/2014
40	300 E	300 S	200 S	16	1	1	0	1	0	0	0	0	0	11/15/2014
42	400 W	400 N	DEAD END	4	0	0	0	0	0	9	0	2	3	11/15/2014
43	500 N	250 E	300 E	4	5	0	0	0	0	9	0	0	0	11/14/2014
44	200 E	400 N	500 N	6	0	0	6	4	0	0	0	0	0	11/15/2014

ID	Road Name	From	To	RSL	Tra	Long	Block	Fatigu	Patch	Edge	Rutting	Rough	Drain	Survey Date
45	500 N	200 E	250 E	6	0	0	0	8	0	0	0	0	3	11/14/2014
46	100 E	400 N	500 N	10	0	0	0	0	0	0	0	2	0	11/15/2014
47	150 E	DEAD END	500 N	0	0	0	0	9	0	0	0	0	3	11/14/2014
48	500 N	150 E	200 E	0	0	0	0	9	0	0	1	0	3	11/14/2014
50	100 W	500 N	600 N	6	0	0	0	5	4	6	0	1	0	11/15/2014
52	100 E	500 N	US HWY	16	0	0	0	0	0	0	0	1	0	11/15/2014
53	350 W	500 N	DIANE DRIVE	4	0	0	0	0	2	9	0	2	0	11/15/2014
54	MAIN STREET	300 S	OLD HWY	8	0	0	0	5	4	0	0	0	3	11/14/2014
55	225 S	CANYON DRIVE	465 E	4	0	0	8	0	0	9	0	0	0	11/14/2014
56	465 E	CITY VIEW	200 S	0	0	0	0	9	0	0	0	0	0	11/14/2014
57	CENTER STREET	200 W	100 W	6	0	0	6	0	4	3	0	0	0	11/14/2014
58	400 W	100 S	CENTE R	4	0	0	0	0	4	9	1	3	3	11/15/2014
59	CENTER STREET	400 W	300 W	6	0	0	6	0	0	0	0	0	3	11/14/2014

ID	Road Name	From	To	RSL	Tra	Long	Block	Fatigu	Patch	Edge	Rutting	Rough	Drain	Survey Date
60	CENTER STREET	500 W	400 W	10	0	0	2	0	4	0	0	2	0	11/14/2014
61	400 W	100 N	200 N	4	2	0	0	0	0	9	0	0	3	11/15/2014
62	200 N	400 W	300 W	8	0	0	5	0	0	2	1	1	0	11/14/2014
63	200 N	500 W	400 W	8	3	0	0	0	0	3	0	1	3	11/14/2014
64	400 E	80 S	40 S	0	0	0	0	9	0	0	0	0	0	11/14/2014
65	300 E	200 N	300 N	8	5	4	0	5	0	0	0	0	0	11/15/2014
66	300 N	300 E	DEAD END	4	0	4	0	6	0	4	1	0	0	11/14/2014
68	100 E	200 N	300 N	16	0	0	0	0	0	0	1	1	0	11/15/2014
69	300 N	100 E	200 E	8	0	0	0	4	0	3	0	1	3	11/14/2014
70	300 N	MAIN STREET	100 E	8	0	0	5	4	0	0	2	2	3	11/14/2014
71	300 N	100 W	MAIN STREE	12	0	0	1	4	0	0	0	0	3	11/14/2014
72	200 W	200 N	300 N	6	0	0	0	0	0	6	0	2	0	11/15/2014
73	300 N	200 W	100 W	6	0	0	6	7	0	0	0	0	3	11/14/2014

ID	Road Name	From	To	RSL	Tra	Long	Block	Fatigu	Patch	Edge	Rutting	Rough	Drain	Survey Date
74	300 W	200 N	300 N	8	2	0	0	0	0	5	0	1	0	11/15/2014
75	300 N	300 W	200 W	8	0	0	0	5	0	0	1	2	3	11/14/2014
76	300 S	80 E	100 E	8	0	0	3	0	0	3	0	0	0	11/14/2014
77	400 W	DEAD END	300 S	4	0	0	0	0	0	9	0	0	0	11/14/2014
78	300 S	100 E	200 E	12	0	0	0	0	2	1	0	0	0	11/14/2014
80	300 W	Old HWY 91	300 S	8	0	0	0	0	2	5	0	0	2	11/14/2014
82	300 S	400 W	300 W	8	5	0	0	0	0	5	0	0	2	11/14/2014
83	200 E	300 N	400 N	10	0	0	4	0	1	0	0	0	0	11/15/2014
84	100 E	300 N	400 N	16	0	0	0	0	0	0	1	1	0	11/15/2014
85	400 N	100 E	200 E	4	0	0	5	0	1	9	0	0	3	11/14/2014
87	400 N	200 E	300 E	8	5	0	0	0	0	3	0	2	0	11/14/2014
88	400 N	MAIN STREET	100 E	4	0	0	0	0	4	9	0	2	3	11/14/2014
89	100 W	300 N	400 N	6	2	0	0	2	4	6	0	0	0	11/15/2014

ID	Road Name	From	To	RSL	Tra	Long	Block	Fatigu	Patch	Edge	Rutting	Rough	Drain	Survey Date
90	400 N	100 W	MAIN STREE	8	0	0	5	0	1	4	0	0	0	11/14/2014
91	300 W	300 N	400 N	10	0	0	0	0	0	4	0	1	3	11/15/2014
92	400 W	300 N	400 N	6	0	0	0	7	0	6	1	0	0	11/15/2014
93	400 N	400 W	300 W	8	0	0	5	0	0	3	1	0	0	11/14/2014
94	200 W	300 N	400 N	6	0	0	0	0	1	8	0	0	3	11/15/2014
95	400 N	200 W	100 W	6	0	0	5	0	0	6	1	0	3	11/14/2014
96	400 N	300 W	200 W	6	0	0	6	0	0	6	0	0	0	11/14/2014
97	400 E	100 S	80 S	0	0	0	0	9	2	0	0	2	0	11/14/2014
98	80 S	400 E	500 E	0	0	0	0	9	3	0	0	2	3	11/14/2014
99	400 W	200 N	300 N	6	0	0	6	0	0	3	0	0	3	11/15/2014
100	300 S	500 W	400 W	6	2	0	0	0	0	8	0	1	2	11/14/2014
101	300 S	300 W	200 W	6	0	0	0	5	0	8	0	0	0	11/14/2014
102	Old HWY 91	100 W	300 S	6	0	0	6	0	1	0	0	1	0	11/14/2014

ID	Road Name	From	To	RSL	Tra	Long	Block	Fatigu	Patch	Edge	Rutting	Rough	Drain	Survey Date
103	100 W	300 S	200 S	8	0	0	0	1	0	5	0	1	0	11/15/2014
104	200 W	300 S	200 S	16	1	0	0	0	1	0	0	1	3	11/15/2014
106	100 E	300 S	200 S	16	0	0	0	0	0	0	0	1	0	11/15/2014
107	200 S	100 E	200 E	10	2	0	0	0	1	2	0	0	3	11/14/2014
108	200 S	MAIN STREET	100 E	8	4	0	0	4	0	3	0	0	3	11/14/2014
109	400 W	300 S	200 S	4	0	0	0	0	4	9	0	2	3	11/15/2014
110	300 W	300 S	200 S	10	0	0	0	1	7	2	0	0	3	11/15/2014
114	125 S	CANYON RD	400 E	4	0	0	0	6	0	0	0	1	1	11/14/2014
115	300 E	200 S	100 S	6	5	0	0	0	0	6	0	0	0	11/15/2014
116	200 E	200 S	100 S	6	0	0	6	0	0	0	0	1	0	11/15/2014
117	100 S	200 E	300 E	20	0	0	0	0	0	0	0	0	3	11/14/2014
118	100 E	200 S	100 S	16	1	0	0	0	0	0	0	0	0	11/15/2014
119	100 S	100 E	200 E	16	0	0	0	0	0	0	0	1	2	11/14/2014

ID	Road Name	From	To	RSL	Tra	Long	Block	Fatigu	Patch	Edge	Rutting	Rough	Drain	Survey Date
121	100 S	MAIN STREET	100 E	16	0	0	0	0	0	0	1	0	3	11/14/2014
122	300 W	200 S	100 S	10	0	0	2	0	0	2	0	0	3	11/15/2014
123	100 S	300 W	200 W	16	0	0	0	0	0	0	0	1	2	11/14/2014
124	200 W	200 S	100 S	16	1	0	0	0	0	0	1	1	3	11/15/2014
125	400 W	200 S	100 S	4	5	0	0	0	8	9	0	0	0	11/15/2014
126	100 S	400 W	300 W	16	0	0	0	0	0	0	0	1	3	11/14/2014
127	100 W	200 S	100 S	8	1	0	0	0	0	5	1	1	0	11/15/2014
128	100 S	500 W	400 W	16	0	0	0	0	0	0	1	0	2	11/14/2014
129	100 S	200 W	100 W	16	0	0	0	0	0	0	1	1	2	11/14/2014
130	400 E	125 S	100 S	0	0	0	0	9	0	0	0	1	2	11/14/2014
131	500 E	80 S	40 S	0	0	0	0	9	2	0	0	0	0	11/14/2014
132	40 S	400 E	500 E	0	0	0	0	9	0	0	0	0	3	11/14/2014
133	300 E	100 S	Center St.	6	0	5	0	8	0	0	0	0	0	11/15/2014

ID	Road Name	From	To	RSL	Tra	Long	Block	Fatigu	Patch	Edge	Rutting	Rough	Drain	Survey Date
134	200 E	100 S	Center St.	6	0	0	6	0	4	0	0	0	0	11/15/2014
135	100 E	100 S	Center St.	8	0	0	0	0	1	7	0	0	2	11/15/2014
137	300 W	100 S	CENTE R	8	3	0	0	0	0	7	0	2	0	11/15/2014
138	100 W	100 S	Center St.	8	0	0	5	0	3	0	0	0	0	11/15/2014
139	CENTER STREET	300 W	200 W	6	0	0	6	0	0	0	0	1	3	11/14/2014
140	200 W	100 S	CENTE R	10	0	0	0	0	1	2	0	1	3	11/15/2014
141	100 W	Center St.	50 N	6	5	0	0	0	0	8	0	0	0	11/15/2014
142	50 N	100 W	MAIN STREE	0	0	0	0	9	6	0	0	0	0	11/14/2014
143	100 E	Center St.	60 N	10	0	0	0	0	0	4	0	2	0	11/15/2014
144	60 N	MAIN STREET	100 E	4	0	0	0	6	4	0	0	0	3	11/14/2014
146	100 W	50 N	100 N	4	5	0	0	0	5	9	0	0	0	11/15/2014
147	100 N	100 W	MAIN STREE	16	0	0	0	0	1	0	0	0	3	11/14/2014
148	200 E	Center St.	100 N	6	0	0	6	0	4	0	1	0	0	11/15/2014

ID	Road Name	From	To	RSL	Tra	Long	Block	Fatigu	Patch	Edge	Rutting	Rough	Drain	Survey Date
149	100 N	200 E	300 E	6	0	0	0	0	1	6	0	0	3	11/14/2014
150	100 E	60 N	100 N	10	0	0	0	0	0	4	0	2	0	11/15/2014
151	100 N	100 E	200 E	6	0	0	0	0	0	6	0	0	3	11/14/2014
152	100 N	MAIN STREET	100 E	4	0	0	0	6	0	0	0	0	3	11/14/2014
153	200 W	CENTER STREET	100 N	8	1	0	0	0	0	5	0	0	2	11/15/2014
154	100 N	200 W	100 W	20	0	0	0	0	0	0	0	0	3	11/14/2014
155	300 W	CENTER STREET	100 N	8	3	0	0	0	7	3	0	1	0	11/15/2014
156	100 N	300 W	200 W	6	0	0	6	0	1	0	0	1	3	11/14/2014
157	400 W	CENTER STREET	100 N	6	0	0	0	0	1	8	0	2	3	11/15/2014
158	100 N	400 W	300 W	6	0	0	6	0	0	8	0	1	3	11/14/2014
159	100 N	500 W	400 W	6	0	0	6	0	0	3	0	0	2	11/14/2014
160	300 E	100 N	200 N	8	5	1	0	5	0	0	0	0	0	11/15/2014
161	200 N	300 W	OLD PARAG	16	0	0	0	0	1	0	0	1	3	11/14/2014

ID	Road Name	From	To	RSL	Tra	Long	Block	Fatigu	Patch	Edge	Rutting	Rough	Drain	Survey Date
162	200 E	100 N	200 N	6	0	0	6	0	0	0	1	1	0	11/15/2014
163	200 N	200 E	300 E	10	0	0	0	0	0	4	0	0	3	11/14/2014
164	100 E	100 N	200 N	10	0	0	0	0	0	0	2	1	0	11/15/2014
165	200 N	100 E	200 E	16	0	0	0	0	1	0	0	0	3	11/14/2014
167	200 W	100 N	200 N	8	0	0	0	0	0	7	1	1	3	11/15/2014
168	200 N	MAIN STREET	100 E	16	0	0	0	0	1	0	0	0	3	11/14/2014
169	300 W	100 N	200 N	10	0	0	2	0	0	2	0	1	0	11/15/2014
170	200 N	300 W	200 W	8	1	0	0	0	0	3	0	0	3	11/14/2014
171	200 E	200 N	300 N	8	0	0	5	0	4	0	0	0	0	11/15/2014
172	300 N	200 E	300 E	8	0	0	0	5	1	3	0	0	0	11/14/2014
173	300 N	500 W	400 W	8	0	0	3	0	1	5	0	0	0	11/14/2014
174	400 N	400 E	DEAD END	10	0	4	0	7	0	0	0	0	0	11/14/2014
176	400 N	300 E	400 W	10	2	5	0	0	4	0	0	0	3	11/14/2014

ID	Road Name	From	To	RSL	Tra	Long	Block	Fatigu	Patch	Edge	Rutting	Rough	Drain	Survey Date
177	400 N	500 W	400 W	8	0	0	5	0	1	3	0	0	0	11/14/2014
178	200 W	400 N	DEAD END	8	0	0	0	0	0	3	0	0	0	11/15/2014
179	500 N	100 E	150 E	4	0	0	0	8	0	9	2	0	0	11/14/2014
181	500 N	MAIN STREET	100 E	4	0	0	0	5	0	9	0	0	3	11/14/2014
182	100 W	400 N	500 N	6	5	0	0	1	4	6	0	2	0	11/15/2014
183	500 N	100 W	MAIN STREE	6	5	0	0	0	0	8	0	0	3	11/14/2014
185	300 E	500 N	600 N	10	0	4	0	0	3	0	0	0	0	11/15/2014
186	250 E	500 N	600 N	10	1	0	0	0	0	2	0	0	0	11/15/2014
187	200 E	500 N	600 N	8	3	0	0	2	2	0	0	0	0	11/15/2014
188	Old HWY 91	300 W	200 W	6	0	0	6	0	1	6	0	0	0	11/14/2014
189	Old HWY 91	200 W	100 W	6	0	0	6	0	0	6	0	0	0	11/14/2014
190	500 W	300 N	400 N	4	0	0	5	6	3	0	1	0	0	11/15/2014
191	500 W	200 N	300 N	6	0	0	0	0	0	8	1	2	3	11/15/2014

ID	Road Name	From	To	RSL	Tra	Long	Block	Fatigu	Patch	Edge	Rutting	Rough	Drain	Survey Date
213	1200 W	DEAD END	375 S	12	4	0	0	4	0	0	0	0	0	11/14/2014
214	375 S	1375 W	1200 W	12	4	0	0	1	0	0	0	0	3	11/14/2014
215	1200 W	375 S	300 S	6	8	0	0	1	0	0	0	0	3	11/14/2014
216	Parowan Heritage Park	DEAD END	Old HWY	6	8	0	0	7	0	0	0	0	3	11/15/2014
218	1000 W	375 S	275 S	10	5	0	0	4	4	0	0	0	0	11/14/2014
220	MAIN STREET	DEAD END	300 S	10	0	0	0	4	2	0	0	2	0	11/14/2014
224	60 N	775 W	725 W	4	0	0	0	6	4	0	0	1	3	11/14/2014
225	725 W	60 N	100 N	6	0	0	0	3	0	0	0	1	3	11/14/2014
226	775 W	60 N	100 N	6	0	0	0	3	0	0	0	1	3	11/14/2014
227	100 N	775 W	725 W	6	0	0	0	3	0	0	0	0	3	11/14/2014
228	200 N	OLD PARAGO	END OF	10	0	0	0	0	0	2	1	0	3	11/14/2014
229	560 W	200 N	400 N	8	0	0	0	0	0	7	0	2	2	11/14/2014
229	560 W	200 N	400 N	8	0	0	0	0	0	7	0	2	2	11/14/2014

[illegible]

ID	Road Name	From	To	RSL	Tra	Long	Block	Fatigu	Patch	Edge	Rutting	Rough	Drain	Survey Date
293	325 S	1400 W	1375 W	14	2	0	0	0	0	0	0	0	0	11/14/2014
294	1375 W	325 S	275 S	10	2	0	0	2	7	0	0	0	0	11/14/2014
295	1375 W	375 S	325 S	8	5	4	0	0	7	3	0	0	0	11/14/2014
296	1375 W	DEAD END	375 S	14	2	0	0	1	0	0	0	0	0	11/14/2014
297	1050 W	575 S	Old HWY	10	5	0	0	4	7	0	0	0	0	11/14/2014
298	575 S	1150 W	1050 W	8	5	0	0	5	0	0	0	0	0	11/14/2014
299	1200 W	LANDFIL L	Old HWY	6	0	0	0	0	7	6	0	1	0	11/14/2014
300	2200 W	475 S	450 S	20	0	0	0	0	0	0	0	0	2	11/14/2014
302	Old HWY 91	1200 W	1150 W	8	0	0	5	0	0	0	0	0	0	11/14/2014
303	Old HWY 91	1525 W	1200 W	10	0	0	2	0	0	0	0	0	2	11/14/2014
304	Old HWY 91	1600 W	1525 W	8	3	0	0	0	2	0	0	0	2	11/14/2014
305	275 S	1025 W	1000 W	10	5	0	0	0	0	0	0	0	0	11/14/2014
306	275 S	1075 W	1025 W	10	5	0	0	1	0	0	0	0	0	11/14/2014

[illegible]

ID	Road Name	From	To	RSL	Tra	Long	Block	Fatigu	Patch	Edge	Rutting	Rough	Drain	Survey Date
325	Fariground	City View Dr.	DEAD END	0	0	0	0	9	0	0	0	0	0	11/15/2014
326	200 E	600 N	US HWY	8	3	0	0	2	1	5	0	0	2	11/15/2014
333	1075 W	275 S	DEAD END	6	8	0	0	0	1	0	0	0	0	11/14/2014
334	1025 W	275 S	DEAD END	10	0	0	4	0	0	0	0	2	0	11/14/2014
335	375 S	1200 W	DEAD END	6	8	5	0	0	0	0	0	0	0	11/14/2014
336	275 S	1375 W	DEAD END	10	5	0	0	1	0	0	0	0	0	11/14/2014
337	325 S	1375 W	DEAD END	10	5	0	0	4	7	0	0	0	0	11/14/2014
338	600 W	CENTER STREET	100 N	8	5	0	0	0	0	3	0	0	3	11/15/2014
339	60 N	725 W	600 W	4	0	0	0	6	0	0	1	0	3	11/14/2014
341	N. Airport Rd.	50 E	END OF	0	0	0	0	9	7	0	0	0	0	11/15/2014
342	1200 W	275 S	200 S	10	5	0	0	4	4	0	0	0	0	11/14/2014
344	1375 W	275 S	200 S	10	5	0	0	4	0	0	0	0	3	11/14/2014
348	1000 W	275 S	200 S	10	5	0	0	0	4	0	0	0	2	11/14/2014

ID	Road Name	From	To	RSL	Tra	Long	Block	Fatigu	Patch	Edge	Rutting	Rough	Drain	Survey Date
350	800 W	DEAD END	200 S	6	0	0	0	3	2	0	0	0	3	11/14/2014
352	750 W	DEAD END	200 S	10	2	0	0	0	1	0	1	2	3	11/14/2014
353	700 W	DEAD END	200 S	0	0	0	0	9	1	0	0	0	3	11/14/2014
357	600 W	300 S	200 S	12	0	1	0	1	4	0	0	0	0	11/14/2014
358	600 W	200 S	100 S	10	4	0	0	2	0	2	0	0	3	11/15/2014
363	400 N	575 W	500 W	8	0	0	5	0	1	0	1	0	3	11/14/2014
364	400 N	WEST FRONTA	575 W	6	0	0	6	0	0	0	0	1	0	11/14/2014
368	500 W	400 N	450 N	6	0	0	0	0	0	6	0	2	3	11/15/2014
369	500 N	500 W	DIANE DRIVE	12	4	0	0	0	0	0	0	0	0	11/15/2014
370	400 E	DEAD END	400 N	6	0	0	0	8	0	0	0	0	0	11/14/2014
373	Old HWY 91	CITY LIMIT	1810 W	12	0	0	1	0	0	0	0	1	0	11/14/2014
374	Old HWY 91	1810 W	1600 W	10	0	0	1	0	0	4	0	0	0	11/14/2014
375	2200 W	500 S	475 S	12	0	0	0	0	0	1	0	0	3	11/14/2014

<i>ID</i>	<i>Road Name</i>	<i>From</i>	<i>To</i>	<i>RSL</i>	<i>Tra</i>	<i>Long</i>	<i>Block</i>	<i>Fatigu</i>	<i>Patch</i>	<i>Edge</i>	<i>Rutting</i>	<i>Rough</i>	<i>Drain</i>	<i>Survey Date</i>
376	WEST FRONTAGE	CITY BOUND	CITY BOUND	8	0	0	0	0	1	5	0	0	0	11/14/2014

Appendix D

Distress Deterioration Table and Recommended Preservation Strategies

Asphalt

Fatigue_id	Severity & Extent	RSL_Fatigue	Strategy
0	No Fatigue Cracking	20	Routine
1	Low,Low	16	Routine
2	Low, Medium	10	Preventative
3	Low, High	6	Rehabilitation
4	Medium, Low	12	Preventative
5	Medium, Medium	8	Preventative
6	Medium, High	4	Rehabilitation
7	High, Low	10	Preventative
8	High, Medium	6	Rehabilitation
9	High, High	0	Reconstruct

Transverse_id	Severity & Extent	RSL_Transverse	Strategy
0	No Cracking	20	Routine
1	Low,Low	16	Routine
2	Low, Medium	14	Routine
3	Low, High	8	Preventative
4	Medium, Low	12	Preventative
5	Medium, Medium	10	Preventative
6	Medium, High	8	Preventative
7	High, Low	10	Preventative
8	High, Medium	6	Rehabilitation
9	High, High	2	Reconstruct

Longitudinal_id	Severity & Extent	RSL_Longitudinal	Strategy
0	No Cracking	20	Routine
1	Low,Low	16	Routine
2	Low, Medium	12	Preventative
3	Low, High	10	Preventative
4	Medium, Low	12	Preventative
5	Medium, Medium	10	Preventative
6	Medium, High	8	Preventative
7	High, Low	10	Preventative
8	High, Medium	8	Preventative
9	High, High	6	Rehabilitation

Patch_id	Severity & Extent	RSL_Patch	Strategy
0	No Cracking	20	Routine
1	Low,Low	16	Routine
2	Low, Medium	12	Preventative
3	Low, High	10	Preventative
4	Medium, Low	12	Preventative
5	Medium, Medium	10	Preventative
6	Medium, High	8	Preventative
7	High, Low	10	Preventative
8	High, Medium	8	Preventative
9	High, High	6	Rehabilitation

Asphalt

Edge_id	Severity & Extent	RSL_Edge	Strategy
0	No Cracking	20	Routine
1	Low,Low	12	No Maintenance
2	Low, Medium	10	Preventative
3	Low, High	8	Preventative
4	Medium, Low	10	Preventative
5	Medium, Medium	8	Preventative
6	Medium, High	6	Rehabilitation
7	High, Low	8	Preventative
8	High, Medium	6	Rehabilitation
9	High, High	4	Rehabilitation

Block_id	Severity & Extent	RSL_Block	Strategy
0	No Cracking	20	Routine
1	Low,Low	12	Routine
2	Low, Medium	10	Preventative
3	Low, High	8	Preventative
4	Medium, Low	10	Preventative
5	Medium, Medium	8	Preventative
6	Medium, High	6	Rehabilitation
7	High, Low	12	Preventative
8	High, Medium	6	Rehabilitation
9	High, High	2	Reconstruct

Rutting_id	Rating	RSL_Rutting	Strategy
0	No Rutting	20	Routine
1	Low	16	Routine
2	Medium	10	Preventative
3	High	4	Rehabilitation

Roughness_id	Rating	RSL_Roughness	Strategy
0	Smooth	20	Routine
1	Low	16	Routine
2	Medium	10	Preventative
3	High	4	Rehabilitation

Drainage_id	Rating
0	Excellent
1	Good
2	Fair
3	Poor

Appendix E

Recommended Preservation Strategies for Each Street Segment

Recommendations - Asphalt

ID	Road Name	From	To	Class	Treatment	Area(yd^2)	Unit Cost
1	1150 W	575 S	Old HWY 91	Residential	Rehabilitation	3631	\$23,385.78
2	Old HWY 91	600 W	300 W	Residential	Reconstruction	8475	\$105,005.2
3	CENTER STREET	600 W	500 W	Residential	Thin Hot Mix Overlay (<2 in)	1311	\$12,457.66
4	600 W	Old HWY 91	400 S	Residential	Routine Maintenance	1737	\$694.71
5	400 S	600 W	500 W	Residential	Thin Hot Mix Overlay (<2 in)	1042	\$9,899.00
6	600 W	400 S	300 S	Residential	Thin Hot Mix Overlay (<2 in)	1630	\$15,489.22
7	300 S	600 W	500 W	Residential	Rehabilitation	1128	\$7,264.32
8	600 W	100 S	CENTER STREET	Residential	Preventative Maintenance	2503	\$3,454.29
9	600 W	100 N	200 N	Residential	Cold Patch	2236	\$670.93
10	200 N	600 W	500 W	Residential	Crack Seal	1629	\$407.33
11	750 W	130 N	200 N	Residential	Rehabilitation	1832	\$11,795.94
12	200 N	750 W	600 W	Residential	Crack Seal	1812	\$452.97
13	780 W	130 N	200 N	Residential	Reconstruction	1832	\$22,694.35
14	200 N	780 W	750 W	Residential	Crack Seal	923	\$230.83
15	200 N	825 W	780 W	Residential	Crack Seal	1027	\$256.67
16	850 W	DEAD END	200 N	Residential	Thin Hot Mix Overlay (<2 in)	1828	\$17,363.89
17	200 N	850 W	825 W	Residential	Crack Seal	854	\$213.39

ID	Road Name	From	To	Class	Treatment	Area(yd^2)	Unit Cost
18	500 W	400 S	300 S	Residential	Thin Hot Mix Overlay (<2 in)	994	\$9,443.00
19	Old HWY 91	1150 W	1050 W	Minor Arterial	Preventative Maintenance	1901	\$2,623.23
20	Old HWY 91	1050 W	850 W	Minor Arterial	Rehabilitation	4303	\$27,709.18
21	1200 W	275 S	300 S	Residential	Preventative Maintenance	983	\$1,356.08
23	100 N	600 W	500 W	Residential	Rehabilitation	1242	\$7,999.91
24	130 N	780 W	750 W	Residential	Rehabilitation	1089	\$7,012.45
25	200 N	875 W	850 W	Residential	Crack Seal	736	\$183.89
26	200 N	900 W	875 W	Residential	Crack Seal	729	\$182.22
27	Old HWY 91	800 W	750 W	Residential	Rehabilitation	1949	\$12,552.99
28	Old HWY 91	850 W	800 W	Minor Arterial	Preventative Maintenance	933	\$1,287.54
31	300 N	400 W	300 W	Residential	Crack Seal	1318	\$329.58
32	300 E	400 N	500 N	Residential	Rehabilitation	1550	\$9,982.00
34	300 S	200 W	100 W	Residential	Thin Hot Mix Overlay (<2 in)	1202	\$11,419.00
36	300 S	MAIN STREET		80 E	Preventative Maintenance	866	\$1,195.08
37	300 S	100 W	Old HWY 91	Residential	Thin Hot Mix Overlay (<2 in)	1098	\$10,434.16
39	300 S	300 E	CANYON RD	Residential	Reconstruction	2500	\$30,969.50
40	300 E	300 S	200 S	Residential	Routine Maintenance	1768	\$707.20
42	400 W	400 N	DEAD END	Residential	Rehabilitation	737	\$4,744.85
43	500 N	250 E	300 E	Residential	Rehabilitation	771	\$4,963.09

ID	Road Name	From	To	Class	Treatment	Area(yd^2)	Unit Cost
44	200 E	400 N	500 N	Residential	Rehabilitation	1244	\$8,014.22
45	500 N	200 E	250 E	Residential	Rehabilitation	731	\$4,705.49
46	100 E	400 N	500 N	Residential	Rotomill & Overlay (<2 in)	1309	\$15,708.00
47	150 E	DEAD END	500 N	Residential	Reconstruction	828	\$10,258.92
48	500 N	150 E	200 E	Residential	Reconstruction	733	\$9,086.00
50	100 W	500 N	600 N	Residential	Thin Hot Mix Overlay (<2 in)	1309	\$12,435.50
52	100 E	500 N	US HWY 271	Residential	No Maintenance	1402	\$0.00
53	350 W	500 N	DIANE DRIVE	Residential	Rehabilitation	2736	\$17,619.84
54	MAIN STREET	300 S	OLD HWY 91	Residential	Preventative Maintenance	650	\$897.31
55	225 S	CANYON DRIVE	465 E	Residential	Rehabilitation	728	\$4,688.32
56	465 E	CITY VIEW DRIVE	200 S	Residential	Reconstruction	552	\$6,839.28
57	CENTER STREET	200 W	100 W	Residential	Rehabilitation	1536	\$9,891.13
58	400 W	100 S	CENTER STREET	Residential	Rehabilitation	1631	\$10,503.64
59	CENTER STREET	400 W	300 W	Residential	Rehabilitation	1434	\$9,232.82
60	CENTER STREET	500 W	400 W	Residential	Rotomill & Overlay (<2 in)	1307	\$15,680.00
61	400 W	100 N	200 N	Residential	Rehabilitation	1393	\$8,973.06
62	200 N	400 W	300 W	Residential	Preventative Maintenance	1512	\$2,086.56
63	200 N	500 W	400 W	Residential	Preventative Maintenance	1491	\$2,057.12
64	400 E	80 S	40 S	Residential	Reconstruction	1104	\$13,678.56

ID	Road Name	From	To	Class	Treatment	Area(yd^2)	Unit Cost
65	300 E	200 N	300 N	Residential	Preventative Maintenance	1589	\$2,192.67
66	300 N	300 E	DEAD END	Residential	Rehabilitation	1761	\$11,341.55
68	100 E	200 N	300 N	Residential	Routine Maintenance	1318	\$527.33
69	300 N	100 E	200 E	Residential	Crack Seal	1323	\$330.75
70	300 N	MAIN STREET	100 E	Residential	Preventative Maintenance	4227	\$5,832.80
71	300 N	100 W	MAIN STREET	Residential	Preventative Maintenance	3071	\$4,238.13
72	200 W	200 N	300 N	Residential	Thin Hot Mix Overlay (<2 in)	1318	\$12,524.16
73	300 N	200 W	100 W	Residential	Rehabilitation	2618	\$16,858.49
74	300 W	200 N	300 N	Residential	Thin Hot Mix Overlay (<2 in)	1436	\$13,644.11
75	300 N	300 W	200 W	Residential	Preventative Maintenance	1311	\$1,809.64
76	300 S	80 E	100 E	Residential	Crack Seal	276	\$69.00
77	400 W	DEAD END	300 S	Residential	Rehabilitation	472	\$3,041.83
78	300 S	100 E	200 E	Residential	No Maintenance	1316	\$0.00
80	300 W	Old HWY 91	300 S	Residential	Thin Hot Mix Overlay (<2 in)	1577	\$14,979.39
82	300 S	400 W	300 W	Residential	Thin Hot Mix Overlay (<2 in)	1182	\$11,231.11
83	200 E	300 N	400 N	Residential	Preventative Maintenance	1262	\$1,741.87
84	100 E	300 N	400 N	Residential	Routine Maintenance	1318	\$527.33
85	400 N	100 E	200 E	Residential	Rehabilitation	1393	\$8,973.06
87	400 N	200 E	300 E	Residential	Crack Seal	1386	\$346.50

ID	Road Name	From	To	Class	Treatment	Area(yd^2)	Unit Cost
88	400 N	MAIN STREET	100 E	Residential	Rehabilitation	4438	\$28,580.72
89	100 W	300 N	400 N	Residential	Thin Hot Mix Overlay (<2 in)	1323	\$12,568.50
90	400 N	100 W	MAIN STREET	Residential	Preventative Maintenance	2144	\$2,958.11
91	300 W	300 N	400 N	Residential	Cold Patch	1439	\$431.63
92	400 W	300 N	400 N	Residential	Thin Hot Mix Overlay (<2 in)	1366	\$12,981.22
93	400 N	400 W	300 W	Residential	Crack Seal	1758	\$439.44
94	200 W	300 N	400 N	Residential	Rotomill & Thick Overlay (3 in.)	1318	\$20,104.58
95	400 N	200 W	100 W	Residential	Thin Hot Mix Overlay (<2 in)	1820	\$17,290.00
96	400 N	300 W	200 W	Residential	Thin Hot Mix Overlay (<2 in)	1764	\$16,758.00
97	400 E	100 S	80 S	Residential	Reconstruction	1144	\$14,174.16
98	80 S	400 E	500 E	Residential	Reconstruction	1412	\$17,494.68
99	400 W	200 N	300 N	Residential	Rehabilitation	1374	\$8,847.13
100	300 S	500 W	400 W	Residential	Rotomill & Thick Overlay (3 in.)	1191	\$18,157.67
101	300 S	300 W	200 W	Residential	Rotomill & Thick Overlay (3 in.)	1120	\$17,080.00
102	Old HWY 91	100 W	300 S	Residential	Rehabilitation	2507	\$16,142.94
103	100 W	300 S	200 S	Residential	Thin Hot Mix Overlay (<2 in)	960	\$9,120.00
104	200 W	300 S	200 S	Residential	Routine Maintenance	1193	\$477.16
106	100 E	300 S	200 S	Residential	No Maintenance	1273	\$0.00
107	200 S	100 E	200 E	Residential	No Maintenance	1923	\$0.00

ID	Road Name	From	To	Class	Treatment	Area(yd^2)	Unit Cost
108	200 S	MAIN STREET	100 E	Residential	Crack Seal	2087	\$521.67
109	400 W	300 S	200 S	Residential	Rehabilitation	933	\$6,009.24
110	300 W	300 S	200 S	Residential	No Maintenance	1257	\$0.00
114	125 S	CANYON RD	400 E	Residential	Rehabilitation	856	\$5,509.78
115	300 E	200 S	100 S	Residential	Thin Hot Mix Overlay (<2 in)	1309	\$12,438.66
116	200 E	200 S	100 S	Residential	Rehabilitation	1104	\$7,112.62
118	100 E	200 S	100 S	Residential	Routine Maintenance	1260	\$503.96
119	100 S	100 E	200 E	Residential	No Maintenance	1271	\$0.00
121	100 S	MAIN STREET	100 E	Residential	Routine Maintenance	2090	\$836.00
122	300 W	200 S	100 S	Residential	No Maintenance	1260	\$0.00
123	100 S	300 W	200 W	Residential	No Maintenance	1128	\$0.00
124	200 W	200 S	100 S	Residential	Routine Maintenance	1162	\$464.80
125	400 W	200 S	100 S	Residential	Rehabilitation	1162	\$7,483.28
126	100 S	400 W	300 W	Residential	No Maintenance	1120	\$0.00
127	100 W	200 S	100 S	Residential	Thin Hot Mix Overlay (<2 in)	1283	\$12,187.45
128	100 S	500 W	400 W	Residential	Routine Maintenance	1124	\$449.60
129	100 S	200 W	100 W	Residential	Routine Maintenance	1200	\$480.00
130	400 E	125 S	100 S	Residential	Reconstruction	1023	\$12,672.22
131	500 E	80 S	40 S	Residential	Reconstruction	1072	\$13,282.08

ID	Road Name	From	To	Class	Treatment	Area(yd^2)	Unit Cost
132	40 S	400 E	500 E	Residential	Reconstruction	1804	\$22,354.31
133	300 E	100 S	Center St.	Residential	Rehabilitation	2911	\$18,747.56
134	200 E	100 S	Center St.	Residential	Rehabilitation	1560	\$10,046.40
135	100 E	100 S	Center St.	Residential	Cold Patch	6912	\$2,073.70
137	300 W	100 S	CENTER STREET	Residential	Preventative Maintenance	1789	\$2,468.67
138	100 W	100 S	Center St.	Residential	Preventative Maintenance	5048	\$6,966.70
139	CENTER STREET	300 W	200 W	Residential	Rehabilitation	1429	\$9,199.90
140	200 W	100 S	CENTER STREET	Residential	No Maintenance	1633	\$0.00
141	100 W	Center St.	50 N	Residential	Rotomill & Thick Overlay (3 in.)	1272	\$19,401.39
142	50 N	100 W	MAIN STREET	Residential	Reconstruction	2519	\$31,210.41
143	100 E	Center St.	60 N	Residential	Cold Patch	1333	\$400.00
144	60 N	MAIN STREET	100 E	Residential	Rehabilitation	1470	\$9,466.80
146	100 W	50 N	100 N	Residential	Rehabilitation	747	\$4,812.11
147	100 N	100 W	MAIN STREET	Residential	Routine Maintenance	4587	\$1,834.67
148	200 E	Center St.	100 N	Residential	Rehabilitation	1673	\$10,776.26
149	100 N	200 E	300 E	Residential	Thin Hot Mix Overlay (<2 in)	1507	\$14,313.34
150	100 E	60 N	100 N	Residential	Cold Patch	756	\$226.67
151	100 N	100 E	200 E	Residential	Thin Hot Mix Overlay (<2 in)	1531	\$14,541.34
152	100 N	MAIN STREET	100 E	Residential	Rehabilitation	4389	\$28,265.16

ID	Road Name	From	To	Class	Treatment	Area(yd^2)	Unit Cost
153	200 W	CENTER STREET	100 N	Residential	Thin Hot Mix Overlay (<2 in)	1757	\$16,691.50
155	300 W	CENTER STREET	100 N	Residential	Preventative Maintenance	1952	\$2,694.37
156	100 N	300 W	200 W	Residential	Rehabilitation	1240	\$7,985.60
157	400 W	CENTER STREET	100 N	Residential	Rotomill & Thick Overlay (3 in.)	1870	\$28,517.50
158	100 N	400 W	300 W	Residential	Rotomill & Thick Overlay (3 in.)	1258	\$19,181.11
159	100 N	500 W	400 W	Residential	Rehabilitation	1242	\$7,999.91
160	300 E	100 N	200 N	Residential	Preventative Maintenance	1603	\$2,211.83
161	200 N	300 W	OLD PARAGONAH	Residential	Routine Maintenance	2289	\$915.56
162	200 E	100 N	200 N	Residential	Rehabilitation	1260	\$8,114.40
163	200 N	200 E	300 E	Residential	Cold Patch	1384	\$415.07
164	100 E	100 N	200 N	Residential	Preventative Maintenance	1335	\$1,841.84
165	200 N	100 E	200 E	Residential	Routine Maintenance	1401	\$560.27
167	200 W	100 N	200 N	Residential	Cold Patch	1332	\$399.70
168	200 N	MAIN STREET	100 E	Residential	Routine Maintenance	1538	\$615.02
169	300 W	100 N	200 N	Residential	No Maintenance	1446	\$0.00
170	200 N	300 W	200 W	Residential	Crack Seal	1485	\$371.33
171	200 E	200 N	300 N	Residential	Preventative Maintenance	1260	\$1,738.80
172	300 N	200 E	300 E	Residential	Preventative Maintenance	1323	\$1,825.74
173	300 N	500 W	400 W	Residential	Thin Hot Mix Overlay (<2 in)	1314	\$12,479.84

ID	Road Name	From	To	Class	Treatment	Area(yd^2)	Unit Cost
174	400 N	400 E	DEAD END	Residential	Preventative Maintenance	1500	\$2,070.00
176	400 N	300 E	400 W	Residential	Preventative Maintenance	3100	\$4,278.00
177	400 N	500 W	400 W	Residential	Crack Seal	1764	\$441.00
178	200 W	400 N	DEAD END	Residential	Crack Seal	611	\$152.83
179	500 N	100 E	150 E	Residential	Rehabilitation	724	\$4,659.70
181	500 N	MAIN STREET	100 E	Residential	Rehabilitation	1552	\$9,996.31
182	100 W	400 N	500 N	Residential	Thin Hot Mix Overlay (<2 in)	1314	\$12,479.84
183	500 N	100 W	MAIN STREET	Residential	Rotomill & Thick Overlay (3 in.)	1692	\$25,796.23
185	300 E	500 N	600 N	Residential	Preventative Maintenance	1519	\$2,096.83
186	250 E	500 N	600 N	Residential	No Maintenance	1531	\$0.00
187	200 E	500 N	600 N	Residential	Preventative Maintenance	1224	\$1,689.73
188	Old HWY 91	300 W	200 W	Residential	Thin Hot Mix Overlay (<2 in)	2097	\$19,920.44
189	Old HWY 91	200 W	100 W	Residential	Thin Hot Mix Overlay (<2 in)	1814	\$17,230.89
190	500 W	300 N	400 N	Residential	Rehabilitation	1295	\$8,339.80
191	500 W	200 N	300 N	Residential	Rotomill & Thick Overlay (3 in.)	1325	\$20,211.33
192	500 W	100 N	200 N	Residential	Rehabilitation	1328	\$8,550.18
193	500 W	CENTER STREET	100 N	Residential	Rotomill & Thick Overlay (3 in.)	1785	\$27,221.25
194	500 W	100 S	CENTER STREET	Residential	Rehabilitation	1638	\$10,548.72
195	500 W	200 S	100 S	Residential	Thin Hot Mix Overlay (<2 in)	1150	\$10,928.16

ID	Road Name	From	To	Class	Treatment	Area(yd^2)	Unit Cost
196	500 W	300 S	200 S	Residential	Rotomill & Thick Overlay (3 in.)	1203	\$18,340.67
198	300 E	600 N	UT HWY 271	Residential	Preventative Maintenance	1072	\$1,479.67
199	100 W	600 N	700 N	Residential	Rotomill & Thick Overlay (3 in.)	912	\$13,913.08
201	700 N	100 W	Main St.	Residential	Preventative Maintenance	1888	\$2,605.44
202	600 N	100 W	MAIN STREET	Residential	Thin Hot Mix Overlay (<2 in)	1529	\$14,524.45
203	600	200 E	250 E	Residential	Preventative Maintenance	728	\$1,004.64
204	600 N	250 E	300 E	Residential	Preventative Maintenance	756	\$1,043.89
205	200 S	200 E	300 E	Residential	Routine Maintenance	1853	\$741.33
212	1000 W	DEAD END	375 S	Residential	Routine Maintenance	568	\$227.11
213	1200 W	DEAD END	375 S	Residential	Preventative Maintenance	1045	\$1,442.10
214	375 S	1375 W	1200 W	Residential	Preventative Maintenance	5305	\$7,320.90
215	1200 W	375 S	300 S	Residential	Rehabilitation	1420	\$9,144.80
216	Parowan Heritage Park	DEAD END	Old HWY 91	Residential	Rehabilitation	3140	\$20,221.60
218	1000 W	375 S	275 S	Residential	Preventative Maintenance	2729	\$3,765.87
220	MAIN STREET	DEAD END	300 S	Residential	Rotomill & Overlay (<2 in)	737	\$8,841.33
224	60 N	775 W	725 W	Residential	Rehabilitation	2089	\$13,452.44
225	725 W	60 N	100 N	Residential	Rehabilitation	1305	\$8,404.20
226	775 W	60 N	100 N	Residential	Rehabilitation	1345	\$8,661.80
227	100 N	775 W	725 W	Residential	Rehabilitation	1036	\$6,672.55

ID	Road Name	From	To	Class	Treatment	Area(yd^2)	Unit Cost
228	200 N	OLD PARAGONAH	END OF PAVEMENT	Residential	No Maintenance	3119	\$0.00
229	560 W	200 N	400 N	Residential	Cold Patch	3749	\$1,124.70
229	560 W	200 N	400 N	Residential	Cold Patch	3749	\$1,124.70
232	500 N	DIANE DRIVE	425 W	Residential	Preventative Maintenance	1247	\$1,720.40
233	500 N	425 W	350 W	Residential	Preventative Maintenance	1188	\$1,639.44
234	50 W	700 N	DEAD END	Residential	Preventative Maintenance	1089	\$1,503.28
241	1075 W	375 S	275 S	Residential	Preventative Maintenance	2244	\$3,096.57
242	850 W	DEAD END	Old HWY 91	Residential	Reconstruction	1219	\$15,108.91
243	1125 W	275 S	DEAD END	Residential	Rehabilitation	696	\$4,482.96
244	1175 W	275 S	DEAD END	Residential	Preventative Maintenance	719	\$992.83
245	1175 W	DEAD END	275 S	Residential	Preventative Maintenance	681	\$939.17
246	1125 W	DEAD END	275 S	Residential	Rotomill & Overlay (<2 in)	712	\$8,540.00
248	300 E	Center St.	100 N	Residential	Preventative Maintenance	2200	\$3,036.00
249	CENTER STREET	100 W	MAIN STREET	Residential	Rehabilitation	3713	\$23,912.44
252	600 W	300 N	DEAD END	Residential	Preventative Maintenance	1400	\$1,932.00
253	325 N	560 W	600 W	Residential	Rehabilitation	2911	\$18,747.56
254	900 W	DEAD END	200 N	Residential	Rehabilitation	1841	\$11,855.33
255	825 W	DEAD END	200 N	Residential	Preventative Maintenance	1599	\$2,206.16
256	100 N	725 W	DEAD END	Residential	Preventative Maintenance	633	\$874.00

ID	Road Name	From	To	Class	Treatment	Area(yd^2)	Unit Cost
257	875 W	DEAD END	200 N	Residential	Reconstruction	2820	\$34,934.30
258	300 N	560 W	600 W	Residential	Preventative Maintenance	1120	\$1,545.60
259	300 N	560 W	500 W	Residential	Preventative Maintenance	1382	\$1,907.47
260	560 W	300 N	DEAD END	Residential	Preventative Maintenance	680	\$938.40
262	200 E	300 S	200 S	Residential	Routine Maintenance	1069	\$427.56
263	750 W	END PAVEMENT	Old HWY	Residential	Rehabilitation	5269	\$33,930.22
266	Old HWY 91	750W	600 W	Residential	Rehabilitation	3077	\$19,815.16
268	Heritage Hills Drive	END OF PAVEMENT	Pinion Circle	Residential	Preventative Maintenance	1178	\$1,626.10
269	Pinion Circle	DEAD END	1525 W	Residential	Preventative Maintenance	1886	\$2,602.83
270	1525 W	Old HWY 91	750 S	Residential	Routine Maintenance	2147	\$858.67
271	725 S	1600 W	DEAD END	Residential	Preventative Maintenance	1505	\$2,076.90
273	Fairground	Loop	Loop	Residential	Reconstruction	3503	\$43,406.30
274	Fairground	300 E	Loop	Residential	Reconstruction	2313	\$28,662.20
275	RED HILLS CIRCLE	465 E	DEAD END	Residential	Reconstruction	716	\$8,871.24
281	N. Airport Rd.	700 N	1000 N	Residential	Reconstruction	5003	\$61,984.42
282	Old HWY 91	300 S	Main St.	Residential	Crack Seal	1020	\$255.00
283	200 S	300 E	CANYON RD	Residential	Crack Seal	989	\$247.22
286	125 S	400 E	DEAD END	Residential	Reconstruction	1447	\$17,924.20
287	225 SOUTH	465 E	500 E	Residential	Rehabilitation	1439	\$9,265.73

ID	Road Name	From	To	Class	Treatment	Area(yd^2)	Unit Cost
290	950 S	1600 W	END OF PAVEMENT	Residential	No Maintenance	246	\$0.00
291	1600 W	950 S	725 S	Residential	Preventative Maintenance	6599	\$9,107.08
292	1600 W	725 S	Old HW Y 91	Residential	Preventative Maintenance	929	\$1,282.17
293	325 S	1400 W	1375 W	Residential	Routine Maintenance	1700	\$680.00
294	1375 W	325 S	275 S	Residential	Preventative Maintenance	1725	\$2,380.50
295	1375 W	375 S	325 S	Residential	Crack Seal	1615	\$403.75
296	1375 W	DEAD END	375 S	Residential	Routine Maintenance	680	\$272.00
297	1050 W	575 S	Old HWY 91	Residential	Preventative Maintenance	3978	\$5,489.64
298	575 S	1150 W	1050 W	Residential	Preventative Maintenance	1782	\$2,459.77
299	1200 W	LANDFILL	Old HWY 91	Residential	Thin Hot Mix Overlay (<2 in)	2702	\$25,669.00
302	Old HWY 91	1200 W	1150 W	Minor Arterial	Preventative Maintenance	1192	\$1,644.35
303	Old HWY 91	1525 W	1200 W	Minor Arterial	Preventative Maintenance	6851	\$9,453.92
304	Old HWY 91	1600 W	1525 W	Minor Arterial	Preventative Maintenance	1724	\$2,378.51
305	275 S	1025 W	1000 W	Residential	Preventative Maintenance	692	\$955.27
306	275 S	1075 W	1025 W	Residential	Preventative Maintenance	1213	\$1,674.40
307	275 S	1125 W	1075 W	Residential	Preventative Maintenance	1241	\$1,711.97
308	275 S	1175 W	1125 W	Residential	Preventative Maintenance	1241	\$1,711.97
309	275 S	1200 W	1175 W	Residential	Preventative Maintenance	719	\$992.83
310	375 S	1025 W	1000 W	Residential	Preventative Maintenance	688	\$949.90

ID	Road Name	From	To	Class	Treatment	Area(yd^2)	Unit Cost
311	1025 W	375 S	DEAD END	Residential	Rehabilitation	1283	\$8,264.66
312	375 S	1075 W	1025 W	Residential	Preventative Maintenance	1174	\$1,620.73
313	100 S	400 E	DEAD END	Residential	Reconstruction	1334	\$16,526.89
314	50 W	200 S	100 S	Residential	Rotomill & Overlay (<2 in)	731	\$8,770.67
315	100 S	50 W	MAIN STREET	Residential	Routine Maintenance	722	\$288.80
319	700 N	MAIN STREET	AIRPORT ROAD	Minor Collector	Reconstruction	400	\$4,956.00
321	475 S	2300 W	2200 W	Residential	Rehabilitation	2459	\$15,838.10
323	450 S	2300 W	2200 W	Residential	Rehabilitation	2463	\$15,860.29
325	Fairground	City View Dr.	DEAD END	Residential	Reconstruction	2923	\$36,210.47
326	200 E	600 N	US HWY 271	Residential	Preventative Maintenance	1011	\$1,395.33
333	1075 W	275 S	DEAD END	Residential	Rehabilitation	747	\$4,808.53
334	1025 W	275 S	DEAD END	Residential	Rotomill & Overlay (<2 in)	751	\$9,006.67
335	375 S	1200 W	DEAD END	Residential	Rehabilitation	2665	\$17,162.60
336	275 S	1375 W	DEAD END	Residential	Preventative Maintenance	2225	\$3,070.50
337	325 S	1375 W	DEAD END	Residential	Preventative Maintenance	1840	\$2,539.20
338	600 W	CENTER STREET	100 N	Residential	Crack Seal	2713	\$678.22
339	60 N	725 W	600 W	Residential	Rehabilitation	3789	\$24,400.44
341	N. Airport Rd.	50 E	END OF PAVEMENT	Residential	Reconstruction	4150	\$51,418.50
342	1200 W	275 S	200 S	Residential	Preventative Maintenance	2046	\$2,823.48

ID	Road Name	From	To	Class	Treatment	Area(yd^2)	Unit Cost
344	1375 W	275 S	200 S	Residential	Preventative Maintenance	1512	\$2,086.56
348	1000 W	275 S	200 S	Residential	Preventative Maintenance	2480	\$3,422.40
350	800 W	DEAD END	200 S	Residential	Rehabilitation	1908	\$12,286.09
352	750 W	DEAD END	200 S	Residential	Rotomill & Overlay (<2 in)	1896	\$22,757.33
353	700 W	DEAD END	200 S	Residential	Reconstruction	1862	\$23,075.68
357	600 W	300 S	200 S	Residential	Preventative Maintenance	1524	\$2,103.27
358	600 W	200 S	100 S	Residential	Preventative Maintenance	1774	\$2,448.43
363	400 N	575 W	500 W	Residential	Preventative Maintenance	1129	\$1,558.48
364	400 N	WEST FRONTAGE RD	575 W	Residential	Rehabilitation	4808	\$30,966.38
368	500 W	400 N	450 N	Residential	Thin Hot Mix Overlay (<2 in)	714	\$6,780.89
369	500 N	500 W	DIANE DRIVE	Residential	Preventative Maintenance	528	\$728.64
370	400 E	DEAD END	400 N	Residential	Rehabilitation	2433	\$15,670.66
373	Old HWY 91	CITY LIMIT	1810 W	Minor Arterial	Routine Maintenance	8064	\$3,225.60
374	Old HWY 91	1810 W	1600 W	Minor Arterial	Cold Patch	4623	\$1,386.93
375	2200 W	500 S	475 S	Residential	No Maintenance	171	\$0.00
376	WEST FRONTAGE	CITY BOUNDARY	CITY BOUNDARY	Minor Collector	Thin Hot Mix Overlay (<2 in)	8847	\$84,048.62

Appendix F

Preservation Strategies, Treatments, and Associated Costs

Typical Repairs for Asphalt Roads

Routine Maintenance

Type of repair

Crack Seal
Digout and Hot Patch
Fog Coat
High Density Mineral Bond (HA5)

Current Unit Area

Cost per yd²

Cost per Unit Area

Added Service Life
When used RSL 13-18

\$0.30
\$0.45
\$0.45
\$1.44

2
0
2
4

Preventive Maintenance

Type of Repair

Sand Seal
Scrub Seal
Single Chip Seal
Slurry Seal
Microsurfacing

Cost per Unit Area

Added Service Life
When used RSL 10-12

\$0.65
\$1.00
\$1.30
\$1.75
\$2.40

2
5
5
5
7

Deferred Maintenance

Type of Repair

No Action
Revert to Gravel
Patch High Severity Defects

Cost per Unit Area

Added Service Life

\$0.00
\$0.00
\$2.00

0
0
0

Rehabilitation

Type of Repair

HMA (leveling) & Overlay (<2 in.)
Thin Hot Mix Overlay (<2 in.)
Hot Surface Recycling
Rotomill & Overlay (<2 in.)
Bonded Wearing Course
Cold In Place Recycling 2 in with Chip Seal

Cost per Unit Area

Added Service Life
When used RSL 7-9

\$7.50
\$6.75
\$5.00
\$8.40
\$6.00
\$5.00

8
7
7
8
5
5

Reconstruction

Type of Repair

Thick Overlay (3 in.)
Base repair and pavement Replacement
Base Replacement and Pavement Replacement
Cold Recycling & Overlay (3 in.)
Rotomill & Thick Overlay (3 in.)
Cold In Place Recycling (2/2 in)
Full Depth Reclamation and Overlay (3/3 in)

Cost per Unit Area

Added Service Life
When used RSL 0

\$10.00
\$12.00
\$20.00
\$11.15
\$11.00
\$10.30
\$13.25

12
16
20
14
12
15
20

Appendix G

Analysis of Current Pavement Preservation Program

Recommended Allocation (2014-2018)

MAINTENANCE	% SYSTEM	COST	0	1-3	4-6	7-9	10-12	13-15	16-18	19-21
MAINTENANCE TYPE: ROUTINE										
	3.00%	\$3,767								
<- Crack Seal	3.00%		0	0	0	0	0	25	75	0
MAINTENANCE TYPE: PREVENTATIVE										
	10.00%	\$45,198								
<- Single Chip Seal	10.00%		0	0	0	60	40	0	0	0
MAINTENANCE TYPE: REHABILITATION										
	6.00%	\$286,256								
<- Thin Hot Mix Overlay (<2 in)	6.00%		0	0	75	25	0	0	0	0
MAINTENANCE TYPE: RECONSTRUCTION										
	0.00%	\$0								
Annual Cost:	\$335,220.50									
Area Treated:	95,419	Yd^2								

Recommended Allocation (2019-2024)

MAINTENANCE	% SYSTEM	COST	0	1-3	4-6	7-9	10-12	13-15	16-18	19-21
MAINTENANCE TYPE: ROUTINE	2.50%	\$3,139								
<> Crack Seal	2.50%		0	0	0	0	0	25	75	0
MAINTENANCE TYPE: PREVENTATIVE	4.00%	\$18,079								
<> Single Chip Seal	4.00%		0	0	0	60	40	0	0	0
MAINTENANCE TYPE: REHABILITATION	1.00%	\$47,709								
<> Thin Hot Mix Overlay (<2 in)	1.00%		0	0	75	25	0	0	0	0
MAINTENANCE TYPE: RECONSTRUCTION	5.30%	\$379,289								
<> Thick Overlay (3 in.)	5.30%		100	0	0	0	0	0	0	0

Annual Cost: \$448,216.19

Area Treated: 64,282 Yd^2

Recommended Plan Analysis (2014-2018)

Percent of System in each RSL Category

Year	0	1-3	4-6	7-9	10-12	13-15	16-18	19-21	AVG RSL
<u>2014</u>	<u>10.49</u>	<u>0</u>	<u>33.58</u>	<u>21.84</u>	<u>24.72</u>	<u>1.17</u>	<u>6.58</u>	<u>1.62</u>	<u>7.75</u>
<u>2015</u>	<u>10.49</u>	<u>9.69</u>	<u>24.17</u>	<u>16.47</u>	<u>20.45</u>	<u>8.31</u>	<u>7.09</u>	<u>3.33</u>	<u>8</u>
<u>2016</u>	<u>13.721</u>	<u>13.02</u>	<u>16.1</u>	<u>11.46</u>	<u>19.99</u>	<u>13.24</u>	<u>8.01</u>	<u>4.47</u>	<u>8.29</u>
<u>2017</u>	<u>18.060</u>	<u>12.55</u>	<u>9.05</u>	<u>7.97</u>	<u>21.32</u>	<u>16.83</u>	<u>8.99</u>	<u>5.23</u>	<u>8.62</u>
<u>2018</u>	<u>22.242</u>	<u>9.88</u>	<u>3.19</u>	<u>6.09</u>	<u>23.41</u>	<u>19.55</u>	<u>9.91</u>	<u>5.74</u>	<u>8.99</u>

Recommended Plan Analysis (2019-2024)

Percent of System in each RSL Category

Year	0	1-3	4-6	7-9	10-12	13-15	16-18	19-21	AVG RSL
<u>2018</u>	<u>22.24</u>	<u>9.88</u>	<u>3.19</u>	<u>6.09</u>	<u>23.41</u>	<u>19.55</u>	<u>9.91</u>	<u>5.74</u>	8.99
<u>2019</u>	<u>20.233</u>	<u>7.4</u>	<u>2.77</u>	<u>9.56</u>	<u>27.7</u>	<u>17.8</u>	<u>8.84</u>	<u>5.7</u>	9.23
<u>2020</u>	<u>17.4</u>	<u>5.61</u>	<u>3.65</u>	<u>13.31</u>	<u>29.98</u>	<u>16.28</u>	<u>8.11</u>	<u>5.68</u>	9.45
<u>2021</u>	<u>13.969</u>	<u>4.71</u>	<u>5.49</u>	<u>16.56</u>	<u>30.99</u>	<u>15.02</u>	<u>7.61</u>	<u>5.66</u>	9.63
<u>2022</u>	<u>10.238</u>	<u>4.72</u>	<u>7.8</u>	<u>19.07</u>	<u>31.24</u>	<u>14.02</u>	<u>7.28</u>	<u>5.65</u>	9.78
<u>2023</u>	<u>6.5103</u>	<u>5.49</u>	<u>10.17</u>	<u>20.83</u>	<u>31.07</u>	<u>13.24</u>	<u>7.05</u>	<u>5.64</u>	9.88
<u>2024</u>	<u>3.0415</u>	<u>6.8</u>	<u>12.34</u>	<u>21.94</u>	<u>30.7</u>	<u>12.64</u>	<u>6.9</u>	<u>5.64</u>	9.96

Appendix H

Recommended Pavement Preservation Program and Proposed Funding Allocation

Current Allocation (2014-2018)

Year	Percent of System in each RSL Category								AVG RSL
	0	1-3	4-6	7-9	10-12	13-15	16-18	19-21	
<u>2014</u>	<u>10.49</u>	<u>0</u>	<u>33.58</u>	<u>21.84</u>	<u>24.72</u>	<u>1.17</u>	<u>6.58</u>	<u>1.62</u>	<u>7.75</u>
<u>2015</u>	<u>10.49</u>	<u>10.84</u>	<u>27.85</u>	<u>19.9</u>	<u>17.3</u>	<u>4.71</u>	<u>5.21</u>	<u>3.71</u>	<u>7.39</u>
<u>2016</u>	<u>14.104</u>	<u>16.16</u>	<u>23.38</u>	<u>16.13</u>	<u>13.52</u>	<u>6.61</u>	<u>4.99</u>	<u>5.1</u>	<u>7.06</u>
<u>2017</u>	<u>19.491</u>	<u>18.22</u>	<u>19.15</u>	<u>12.36</u>	<u>11.64</u>	<u>7.8</u>	<u>5.31</u>	<u>6.02</u>	<u>6.79</u>
<u>2018</u>	<u>25.564</u>	<u>18.18</u>	<u>15.07</u>	<u>9.22</u>	<u>10.79</u>	<u>8.7</u>	<u>5.83</u>	<u>6.64</u>	<u>6.58</u>
<u>2019</u>	<u>31.624</u>	<u>16.79</u>	<u>11.3</u>	<u>6.84</u>	<u>10.52</u>	<u>9.48</u>	<u>6.38</u>	<u>7.05</u>	<u>6.43</u>
<u>2020</u>	<u>37.222</u>	<u>14.61</u>	<u>8</u>	<u>5.17</u>	<u>10.6</u>	<u>10.18</u>	<u>6.89</u>	<u>7.33</u>	<u>6.33</u>
<u>2021</u>	<u>42.093</u>	<u>12.06</u>	<u>5.24</u>	<u>4.08</u>	<u>10.88</u>	<u>10.82</u>	<u>7.32</u>	<u>7.51</u>	<u>6.29</u>
<u>2022</u>	<u>46.113</u>	<u>9.44</u>	<u>3.04</u>	<u>3.45</u>	<u>11.29</u>	<u>11.38</u>	<u>7.67</u>	<u>7.63</u>	<u>6.28</u>
<u>2023</u>	<u>49.258</u>	<u>6.95</u>	<u>1.36</u>	<u>3.16</u>	<u>11.74</u>	<u>11.88</u>	<u>7.94</u>	<u>7.71</u>	<u>6.31</u>

Percent of System in each RSL Category

Year	0	1-3	4-6	7-9	10-12	13-15	16-18	19-21	AVG RSL
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Current Allocation (2019-2022)

MAINTENANCE	% SYSTEM	COST	0	1-3	4-6	7-9	10-12	13-15	16-18	19-21
<hr/>										
MAINTENANCE TYPE: ROUTINE	3.50%	\$4,394								
<hr/>										
<> Crack Seal	3.50%		0	0	0	0	0	25	75	0
<hr/>										
MAINTENANCE TYPE: PREVENTATIVE	5.00%	\$22,599								
<hr/>										
<> Single Chip Seal	5.00%		0	0	0	60	40	0	0	0
<hr/>										
MAINTENANCE TYPE: REHABILITATION	0.50%	\$23,855								
<hr/>										
<> Thin Hot Mix Overlay (<2 in)	0.50%		0	0	50	50	0	0	0	0
<hr/>										
MAINTENANCE TYPE: RECONSTRUCTION	0.60%	\$42,938								
<hr/>										
<> Thick Overlay (3 in.)	0.60%		0	0	0	0	0	0	0	0
<hr/>										

Annual Cost: \$93,786.42

Area Treated: 48,211 Yd^2

Current Allocation (2023-2024)

MAINTENANCE	% SYSTEM	COST	0	1-3	4-6	7-9	10-12	13-15	16-18	19-21
MAINTENANCE TYPE: ROUTINE										
	3.50%	\$4,394								
<> Crack Seal	3.50%		0	0	0	0	0	25	75	0
MAINTENANCE TYPE: PREVENTATIVE										
	5.00%	\$22,599								
<> Single Chip Seal	5.00%		0	0	0	40	60	0	0	0
MAINTENANCE TYPE: REHABILITATION										
	0.50%	\$23,855								
<> Thin Hot Mix Overlay (<2 in)	0.50%		0	0	50	50	0	0	0	0
MAINTENANCE TYPE: RECONSTRUCTION										
	0.60%	\$42,938								
<> Thick Overlay (3 in.)	0.60%		0	0	0	0	0	0	0	0

Annual Cost: \$93,786.42

Area Treated: 48,211 Yd^2

Current Funding Analysis (2014-2018)

Year	Percent of System in each RSL Category								AVG RSL
	0	1-3	4-6	7-9	10-12	13-15	16-18	19-21	
<u>2014</u>	<u>10.49</u>	<u>0</u>	<u>33.58</u>	<u>21.84</u>	<u>24.72</u>	<u>1.17</u>	<u>6.58</u>	<u>1.62</u>	<u>7.75</u>
<u>2015</u>	<u>10.49</u>	<u>10.84</u>	<u>27.85</u>	<u>19.9</u>	<u>17.3</u>	<u>4.71</u>	<u>5.21</u>	<u>3.71</u>	<u>7.39</u>
<u>2016</u>	<u>14.104</u>	<u>16.16</u>	<u>23.38</u>	<u>16.13</u>	<u>13.52</u>	<u>6.61</u>	<u>4.99</u>	<u>5.1</u>	<u>7.06</u>
<u>2017</u>	<u>19.491</u>	<u>18.22</u>	<u>19.15</u>	<u>12.36</u>	<u>11.64</u>	<u>7.8</u>	<u>5.31</u>	<u>6.02</u>	<u>6.79</u>
<u>2018</u>	<u>25.564</u>	<u>18.18</u>	<u>15.07</u>	<u>9.22</u>	<u>10.79</u>	<u>8.7</u>	<u>5.83</u>	<u>6.64</u>	<u>6.58</u>
<u>2019</u>	<u>31.624</u>	<u>16.79</u>	<u>11.3</u>	<u>6.84</u>	<u>10.52</u>	<u>9.48</u>	<u>6.38</u>	<u>7.05</u>	<u>6.43</u>
<u>2020</u>	<u>37.222</u>	<u>14.61</u>	<u>8</u>	<u>5.17</u>	<u>10.6</u>	<u>10.18</u>	<u>6.89</u>	<u>7.33</u>	<u>6.33</u>
<u>2021</u>	<u>42.093</u>	<u>12.06</u>	<u>5.24</u>	<u>4.08</u>	<u>10.88</u>	<u>10.82</u>	<u>7.32</u>	<u>7.51</u>	<u>6.29</u>
<u>2022</u>	<u>46.113</u>	<u>9.44</u>	<u>3.04</u>	<u>3.45</u>	<u>11.29</u>	<u>11.38</u>	<u>7.67</u>	<u>7.63</u>	<u>6.28</u>
<u>2023</u>	<u>49.258</u>	<u>6.95</u>	<u>1.36</u>	<u>3.16</u>	<u>11.74</u>	<u>11.88</u>	<u>7.94</u>	<u>7.71</u>	<u>6.31</u>

Percent of System in each RSL Category

Year	0	1-3	4-6	7-9	10-12	13-15	16-18	19-21	AVG RSL
------	---	-----	-----	-----	-------	-------	-------	-------	---------

Current Funding Analysis (2019-2022)

Year	Percent of System in each RSL Category							AVG RSL
	0	1-3	4-6	7-9	10-12	13-15	16-18	19-21
<u>2018</u>	<u>25.56</u>	<u>18.18</u>	<u>15.07</u>	<u>9.22</u>	<u>10.79</u>	<u>8.7</u>	<u>5.83</u>	<u>6.64</u>
<u>2019</u>	<u>31.62</u>	<u>17.06</u>	<u>11.87</u>	<u>6.91</u>	<u>9.72</u>	<u>9.41</u>	<u>6.35</u>	<u>7.05</u>
<u>2020</u>	<u>37.306</u>	<u>15.25</u>	<u>8.97</u>	<u>5.01</u>	<u>9.24</u>	<u>10.06</u>	<u>6.83</u>	<u>7.33</u>
<u>2021</u>	<u>42.388</u>	<u>13.07</u>	<u>6.4</u>	<u>3.59</u>	<u>9.14</u>	<u>10.65</u>	<u>7.25</u>	<u>7.51</u>
<u>2022</u>	<u>46.745</u>	<u>10.76</u>	<u>4.21</u>	<u>2.61</u>	<u>9.27</u>	<u>11.18</u>	<u>7.59</u>	<u>7.63</u>

Current Funding Analysis (2023-2024)

Year	Percent of System in each RSL Category								AVG RSL
	0	1-3	4-6	7-9	10-12	13-15	16-18	19-21	
<u>2022</u>	<u>46.75</u>	<u>10.76</u>	<u>4.21</u>	<u>2.61</u>	<u>9.27</u>	<u>11.18</u>	<u>7.59</u>	<u>7.63</u>	6.04
<u>2023</u>	<u>50.336</u>	<u>8.49</u>	<u>2.76</u>	<u>2.33</u>	<u>8.53</u>	<u>11.32</u>	<u>8.52</u>	<u>7.71</u>	6.01
<u>2024</u>	<u>53.167</u>	<u>6.5</u>	<u>1.7</u>	<u>1.9</u>	<u>8.09</u>	<u>11.72</u>	<u>9.17</u>	<u>7.77</u>	6.01

WM12

John Orton Excavating, Inc.

P.O. Box 1689
997 W. 1350 North
Cedar City, UT 84721



Phone # 435-586-6163
Fax # 435-586-1492

joe@netutah.com
ortonexcavating.com

DATE	INVOICE
11/6/2014	7962

TO
West Walker 2431 Ping Dr. Henderson NV 89074

FROM
Attn: West Walker Asphalt Job

P.O. NUMBER	TERMS	DUE DATE
		11/6/2014

QUANTITY	ITEM CODE	DESCRIPTION	PRICE EACH	SERVICES	AMOUNT
1	Bid	Curbing and prep	11,019.51		11,019.51
1	Bid	Dig and Haul off 8' of material for roadbase	3,800.00		3,800.00
1	Bid	Dig and haul off of existing asphalt	1,150.00		1,150.00
1	Bid	Roadbase 6" thick	10,871.25		10,871.25
1	Bid	Asphalt	31,059.60		31,059.60
1	Bid	Landscape Rock (extra)	3,409.72		3,409.72
1	Bid	Build up Ken's side road	270.00		270.00
1	Bid	Raise Water Valve collar with concrete	350.00		350.00
1	credit	discount	-1,500.00		-1,500.00
<p>Asphalt 5600 sq FT @ \$2.12 = \$11,872.00</p> <p>Remove exist. asphalt 1150.00 = \$1150.00</p> <p>5600 sq FT Road Base 6" Thick @ \$0.75 = \$4200.00</p> <p>Curb Gutter sidewalk 290 LF @ \$14.00 = \$4060.00</p> <p>Total spent in existing Sub. Div = \$21,282</p>					
Total					\$60,430.08

Overdue invoices will be charged 1.5% per month
(18% per annum).

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ortonexcavating.com

11/5/2014	7960

Ken Allen
1611 Quarter Horse Dr.
Henderson NV 89002

Ken Allen
Subdivision Water and Sewer

PO NUMBER	ITEMS	DATE
		11/5/2014

QUANTITY	UNIT CODE	DESCRIPTION	PRICE PER	TERMS	AMOUNT
180	Bid	8" Sewer Main with tie-in to existing pipe	24.00	93 LF	4,320.00
1	Bid	1 Manhole approx. 13.5' deep	1,400.00	NA	1,400.00
2	Bid	4" Sewer Lateral per City Standard	1,400.00	NA	2,800.00
160	Bid	8" C900 Water Main	20.00	160 LF	3,200.00
1	Bid	Fire Hydrant	4,100.00	NA	4,100.00
2	Bid	1" Water Lateral per City Standard	1,500.00	NA	3,000.00
1	Bid	Tee and Cap with thrust blocks for each	1,550.00	NA	1,550.00
1	Bid	Extra time for finding Water Main Line	4,147.50	4147.50	4,147.50
1	credit	Discount	-1,000.00		-1,000.00
Total spent in existing sub. Div.				89699.50	
			Total		\$23,517.50

Overdue invoices will be charged 1.5% per month
(18% per annum).

PAROWAN SHADE TREE BUDGET FY 2015

Approved November 6, 2014

INCOME**A. Appropriated Funds**

1. Parowan City (Tree City USA requirement, i.e. \$2/resident)	\$5,000
2. Parowan City Power for powerline tree replacements	\$1,000
3. Community Forestry Partnership Grant for "City Forest Renewal"	\$8,000*
4. Carryover of funds from donations and other	\$2,350
5. Anticipated donations "not earmarked"	\$ 650
Total Anticipated Income	\$17,000
Anticipated Income in F. Y. 2014	\$15,500

EXPENDITURES**A. Purchase of Trees:**

1. Parowan City Nursery (Seedlings)	50 ea.	\$ 200
2. Street plantings for CFP Grant (30 from City Nursery) +	55 ea.	\$6,000*
3. Powerline Street Plantings	8 ea.	\$1,000*
4. Tree planting @ Fairgrounds (5 from City Nursery) +	17 ea.	\$1,573
5. Recent mortality + Main St. replacements+ others	10 ea.	\$1,327
Total Tree Purchases	90 ea.*	\$10,000
Purchases in F. Y. 2014		\$15,500

* Not including seedlings purchased for the City nursery

B. Tree Protection and Maintenance (Including Nursery & Arboretum):

1. Deer Fence, Fence posts, herbicide	\$ 400*
Total Protection & Maintenance	\$ 400
P & M in FY 2014	\$1,900

C. Tree Pruning:

1. Pruning/On-the-job Training for new Arborist (\$400 ea. for 5 trees)	\$2,000*
Total Tree Pruning	\$2,000
Pruning in FY 2014	\$1,900

D. Stump Grinding:

1. Rental of commercial grade grinder @ \$1,000 /week for 2 weeks	\$2,000*
Total Stump Grinding	\$2,000
Stump Grinding in FY 2014	\$1,017

E. Education:

1. Tree Care Bulletins (Fall & Spring Editions)	500 ea.	\$1,000*
2. Arbor Day Fund. memberships and bulletins	6 ea.	\$ 150
3. Printing (fliers, aids, etc.)		\$ 350
Total Education		\$1,500
Education in FY 2014		\$1,200

E. Planning:

1. Purchase of I-Pad or similar tool for up-grading Tree Inventory	\$1,100*
Total Planning	\$1,100
Planning in FY 2014	\$3,000

<u>TOTAL INCOME ANTICIPATED in FY 2015:</u>	(\$15,500 in FY 2014)	\$17,000
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<u>TOTAL EXPENDITURES PLANNED for 2015:</u>	(\$15,500 in FY 2014)	\$17,000
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PAROWAN SHADE TREE BOARD
C. Y. 2014 YEAR-END ACCOMPLISHMENT SUMMARY

TOTAL VALUE OF ALL COMMUNITY FORESTRY PROGRAMS			\$90,374
Value of Last Year's Program			\$72,838
A. <u>Tree Planting Program*</u>			\$13,046
Value of Last Year's Program			\$8,683
Total Trees Planted	101	92	
Purchased	56	80	
City Nursery	45	12	
Different Species	30	22	
Different locations	14	17	
B. <u>Tree Protection & Maintenance*</u>			\$11,477
Value of Last Year's Program			\$12,852
Trees Protected	212	304	
Deer Cages	115	304	
Tree Mulch Rings/Rec's	97	206	
C. <u>Tree Pruning Including line Clearing*</u>			\$14,588
Value of Last Year's Program			\$5,645
Young Tree Pruning	163	359	
Line Clearing			\$11,366
D. <u>Tree P & M including Pruning (B + C)</u>			\$26,065
Value of Last Year's Program			\$18,497
E. <u>Trees Removed</u>	73		\$38,205
Value of Last Year's Program		25	\$2,531
F. <u>Program Management</u>	(Planning, Supervision. & Education)		\$13,058
Value of Last Year's Program			\$16,102
G. <u>Volunteers**</u>	(Value @ USDL \$22.14 & 22.55/Hr.)		\$25,075
Value of Last Year's Program			\$36,132
Hours Expended	1,632	1,390	
H. <u>City Crew Contributions**</u>			\$51,306
Value of Last Year's Program			\$27,390
Public Works			\$35,043
Parowan City Power			\$16,263
Equipment			\$33,030
Wages			\$18,276

*Categories include different components this year. For example cages, initial irrigation and mulch rings are included in **Tree Planting** this year rather than in **Tree Protection & Maintenance**.

**Volunteer and City Crew contributions are included in categories A – E so adding all of the right hand column values will give you a false total.

Parowan City Sanitary Sewer Management Plan

Introduction

Parowan City is a [public entity] established in Utah under the Utah State Code. Parowan City was established in 1968 and provides sewage collection and/or treatment to Parowan City and Brian Head Town. This Sewer System Management Plan (SSMP) manual has been established to provide a plan and schedule to properly manage, operate, and maintain all parts of the sewer collection system to reduce and prevent SSOs, as well as minimize impacts of any SSOs that occur. The Management for this entity recognizes the responsibility it has to operate the sewer system in an environmentally and fiscally responsible manner. As such, this manual will cover aspects of the collection system program necessary to provide such an operation. This manual may refer to other programs or ordinances and by reference may incorporate these programs into this manual.

Definitions

The following definitions are to be used in conjunction with those found in Utah Administrative Code R317. The following terms have the meaning as set forth:

- (1) "BMP" means "best management practice".
- (2) "CCTV" means "closed circuit television".
- (3) "CIP" means a "Capital Improvement Plan".
- (4) "DWQ" means "the Utah Division of Water Quality".
- (5) "FOG" means "fats, oils and grease". This is also referred to as a Grease Oil and Sand Program(GOSI).
- (6) "I/I" means "infiltration and inflow".

(7) "Permittee" means a federal or state agency, municipality, county, district, and other political subdivision [public entity] of the state that owns or operates a sewer collection system or who is in direct responsible charge for operation and maintenance of the sewer collection system. When two separate federal or state agency, municipality, county, district, and other political subdivision of the state are interconnected, each shall be considered a separate Permittee.

(8) "SECAP" means "System Evaluation and Capacity Assurance Plan".

(9) "Sewer Collection System" means a system for the collection and conveyance of wastewaters or sewage from domestic, industrial and commercial sources. The Sewer Collection System does not include sewer laterals under the ownership and control of an owner of real property, private sewer systems owned and operated by an owner of real property, and systems that collect and convey stormwater exclusively.

(10) "SORP" means "Sewer Overflow Response Plan"

(11) "SSMP" means "Sewer System Management Plan".

(12) "SSO" means "sanitary sewer overflow", the escape of wastewater or pollutants from, or beyond the intended or designed containment of a sewer collection system.

(13) "Class 1 SSO" (Significant SSO) means a SSO or backup that is not caused by a private lateral obstruction or problem that:

(a) affects more than five private structures;

(b) affects one or more public, commercial or industrial structure(s);

(c) may result in a public health risk to the general public;

(d) has a spill volume that exceeds 5,000 gallons, excluding those in single private structures; or

(e) discharges to Waters of the State of Utah.

(14) "Class 2 SSO" (Non Significant SSO) means a SSO or backup that is not caused by a private lateral obstruction or problem that does not meet the Class 1 SSO criteria.

(15) "USMP" means the "Utah Sewer Management Program".

General SSO Requirements

The following general requirements for SSO's are stipulated in R317-801 and are included here as general information.

1) The permittee shall take all feasible steps to eliminate SSOs to include:

(a) Properly managing, operating, and maintaining all parts of the sewer collection system;

(b) training system operators;

(c) allocating adequate resources for the operation, maintenance, and repair of its sewer collection system, by establishing a proper rate structure, accounting mechanisms, and auditing procedures to ensure an adequate measure of revenues and expenditures in accordance with generally acceptable accounting practices; and,

(d) providing adequate capacity to convey base flows and peak flows, including flows related to normal wet weather events. Capacity shall meet or exceed the design criteria of R317-3.

(2) SSOs shall be reported in accordance with the requirements below.

(3) When an SSO occurs, the permittee shall take all feasible steps to:

(a) control, contain, or limit the volume of untreated or partially treated wastewater discharged;

(b) terminate the discharge;

(c) recover as much of the wastewater discharged as possible for proper disposal, including any wash down water; and,

(d) mitigate the impacts of the SSO.

SSO Reporting Requirements

R317-801 stipulates when and how SSO's are reported. Following are those reporting requirements as of 04/23/2012.

SSO REPORTING. SSOs shall be reported as follows:

(1) A Class 1 SSO shall be reported orally within 24 hrs and with a written report submitted to the DWQ within five calendar days. Class 1 SSO's shall be included in the annual USMP report.

(2) Class 2 SSOs shall be reported on an annual basis in the USMP annual report.

ANNUAL REPORT. A permittee shall submit to DWQ a USMP annual operating report covering information for the previous calendar year by April 15 of the following year.

Sewer Use Ordinance

Parowan has a sewer use [ordinance, rules, or regulations] that has been adopted by the governing body. This [ordinance or rules] contains the following items as stipulated by Utah State Code R317-801:

1. Prohibition on unauthorized discharges,
2. Requirement that sewers be constructed and maintained in accordance with R317-3,
3. Ensures access or easements for maintenance, inspections and repairs,
4. Has the ability to limit debris which obstruct or inhibit the flow in sewers such as foreign objects or grease and oil,
5. Requires compliance with pretreatment program [delete if no pretreatment program exists],
6. Allows for the inspection of industrial users, and
7. Provides for enforcement of for ordinance or rules violations.

The following elements are included in this SSMP:

- General Information
- Operations and Maintenance Program
- Sewer Design Standards
- Sanitary Sewer Overflow Response Plan
- Grease, Oil and Sand Interceptor Management Program
- System Evaluation and Capacity Assurance Plan
- SSMP Monitoring and Measurement Plan
- Sewer System Mapping Program

This program is intended to be a guidance document and is not intended to be part of a regulatory requirement. As such, failure to strictly comply with documentation requirements is, in and of themselves, not a failure of the program's effectiveness. Documentation failures are intended to be identified during system self-audits and will be addressed as training opportunities. Significant system failures will be followed up with corrective action plans. This corrective action process will be implemented by all individuals involved in the SSMP program. Not all [public entity] employees will necessarily be involved in the collection system operations. As such, not all employees will receive program training. Finally, although not a part of this SSMP program, [public entity] is an active participant in the Blue Stakes of Utah Utility Notification system. This system, regulated under title 54-8A of the Utah State Code, stipulates utility notification of all underground operators when excavation takes place. The intent of this regulation is to minimize damage to underground facilities. [Public entity] has a responsibility to mark their underground sewer facilities when notified an excavation is going to take place. Participation in the Blue Stakes program further enhances the protection of the collection system and reduces SSO's.

Parowan City

SSMP – General Information

The responsible representative(s), position and phone number for Parowan City with regard to this SSMP is/are

Kelly Stones 435-559-3645 _____

Aldo Biasi 435-559-4621 _____

Cobe Evans 435-559-0205 _____

Description of Roles and Responsibilities

The following positions have the described responsibility for implementation and management of the specific measures as described in the SSMP.

Manager

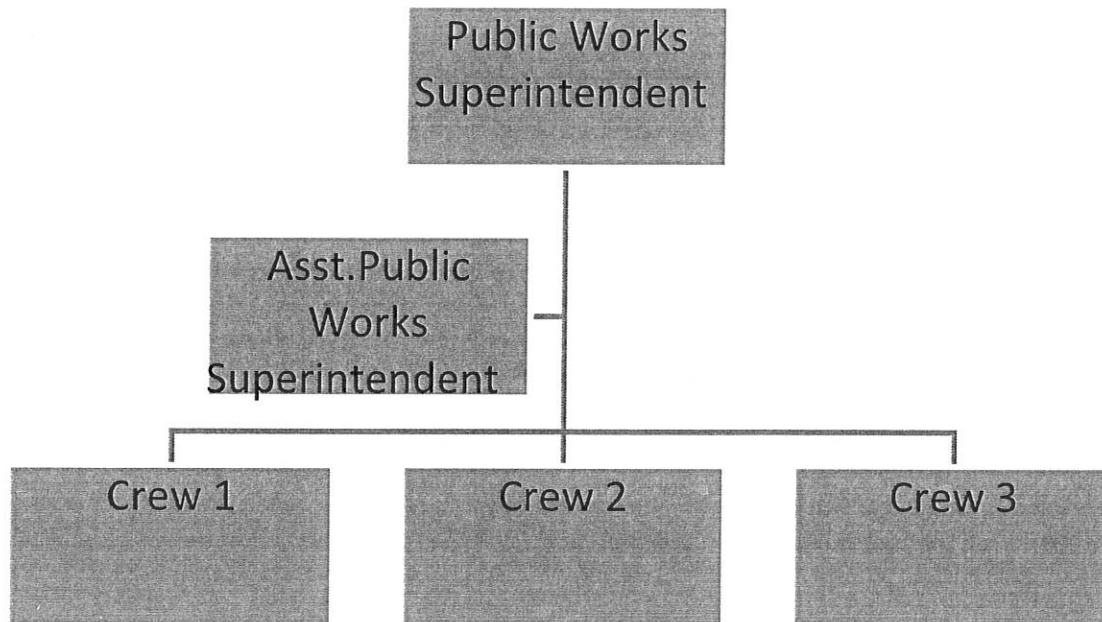
This individual is responsible for overall management of the sanitary sewer collection system. Responsibilities include working with governance to assure sufficient budget is allocated to implement the SSMP, maintenance of the SSMP documentation, development of a capital improvement program and general supervision of all staff.

Superintendent

This individual is responsible for daily implementation of the SSMP. This includes maintenance activities, compliance with SORP requirements, and monitoring and measurement reporting requirements.

Organization Chart

Below is the organization chart associated with the SSMP [this could be a large chart or just one person depending on organization size]:



Parowan City

No-Fault Sewage Backup Claims Program

Purpose:

The purpose of this program is to assist in the cleanup of real and personal property, and/or compensate persons for the loss of real or personal property, destroyed or damaged as the result of a backup of Parowan City facilities, regardless of fault, within the restrictions, limitations and other provisions of this policy.

Cleanup of Real and Personal Property:

- (A) The Public Works may, in accordance with the Parowan City's standard procurement procedures, engage the services of one or more cleanup contractors to perform cleanup services at the direction of the Public Works on an as-needed basis.
- (B) Upon discovering backup described in this Policy, a property owner should immediately notify the Public Works of such event.
- (C) Upon notification of the occurrence of the event, the Public Works may contact a cleanup contractor under contract with the Parowan City pursuant to subsection (A) above, and direct the cleanup contractor to perform all cleanup work at the premises, in accordance with established cleanup criteria.
- (D) In the event the property owner engages the services of a cleanup contractor prior to notifying the Public Works of the event, the Parowan City may reimburse the property owner for actual expenses incurred by the property owner, but only up to the amount the Parowan City would have paid its own cleanup contractor under subsection (C) above.
- (E) In the event any real or personal property cannot, in the reasonable judgment of the Public Works, be restored to its pre-event condition, in accordance with the cleanup criteria, the Parowan City may pay to the property owner the estimated fair market value (not the replacement value) at the time of the event, of such real or personal property, with the exception that carpet and major appliances will be replaced with new like-kind items.
- (F) In no event will the Parowan City pay, or reimburse the property owner for the payment of special or consequential damages.

Establishment of Cleanup Criteria:

The Public Works may, from time to time, establish cleanup criteria which will govern the Parowan City's cleanup and payment responsibilities under this Policy. In establishing such cleanup criteria, the Public Works may give due consideration to generally available health guidelines, recommendations from governmental and academic experts, and other sources of guidance reasonably deemed by the Public Works to be balanced, unbiased, and protective of health and safety.

Application - Time Limitations:

Any request for reimbursement of cleanup expenses under this policy, or payment of fair market value, may be made by filing a written application in such form as prescribed by the Public Works. Such application must be submitted to the Parowan City within thirty (30) days after the occurrence of the event.

Qualification for Assistance:

An application or request for assistance or payment under this Policy may qualify only if Parowan City, after due inquiry or investigation, makes an affirmative determination that the event was the result of a backup of Parowan City facilities, and that none of the following circumstances apply:

- (A) The loss was the result of a force majeure including but not limited to acts of God, acts of public enemies, insurrections, riots, war, landslides, lightning, earthquakes, fires, storms, floods, washouts, droughts, civil disturbances, explosions, acts of terrorism, sabotage, or any other similar cause or event not reasonably within Parowan City's control;
- (B) The loss was caused by either an act or omission of the property owner, the property owner's agent, or a member of the property owner's family or business;
- (C) The property owner failed to file a claim hereunder in a timely manner, or failed to comply with any other procedural requirements of this Policy;
- (D) The loss is the result of intentional or negligent acts of third parties; or
- (E) The loss is wholly covered by private insurance.

Reduction in Assistance:

The Parowan City may limit any assistance, or reduce any payment, under this Policy based upon any of the following:

- (A) The property owner did not act responsibly to prevent, avoid or minimize the loss;
- (B) The property owner is unable to fully substantiate or document the extent of the loss;
- (C) The loss is partially covered by private insurance.

Maximum Payments:

Without the express action of Parowan City Council no assistance or payment under this Policy may exceed any of the following:

- (A) _____ dollars (\$XXXX) per application or location; or
- (B) _____ dollars (\$XXXXXX.XX) per incident.
Should a catastrophic event occur, the \$XXXXXX.XX per incident limitation will be prorated against all losses where assistance is requested unless additional funding is approved by the governing authority.

Payment Does Not Imply Liability:

Any assistance or payment made under this Policy shall not be construed as, and does not imply, an admission of negligence or responsibility on the part of the [public entity] for any damage or loss. Any assistance or payment made under this Policy is strictly voluntary on the part of the Parowan City. This Policy shall not in any way supersede, change or abrogate the state government immunity act, Utah Code Annotated, section 63-30-1 et seq., as amended, or its successor, and its application to the [public entity], or establish in any person a right to sue the Parowan City under this Policy. Any assistance or payment made under this Policy and accepted shall constitute a full and complete release of any and all claims against the Parowan City, its officers, employees and agents arising from the incident.

Budget Expenditures:

The Parowan City authorizes a fund from which amounts may be drawn to make the foregoing assistance or payments. Such fund may be established from the ordinary rate structure of the Parowan City.

Claims from Other Governmental Agencies:

Notwithstanding any other provisions of this Policy, no application shall be accepted from the United States or any of its agencies, the State of Utah or any political subdivision.

PAROWAN CITY COUNCIL 2015 GOALS

(In no priority order – bold items require financial obligation)

1. **Power Department Resource Study (not to exceed \$10,000)**
2. Communication Plan Creation
3. **Splash Pad Donation Project (Power Department and Community)**
4. **Trails – Mountain Bike Park**
5. **Tennis Courts**
6. **Event Additions - Focus on Softball/Baseball/Soccer/Rodeo Arena**
7. Branding/Marketing Plan and Implementation
8. **Exit 75 Realignment**
9. Recreation Master Plan
10. Go Through Engineering Standards/Ordinances – Impact Fee Credits
11. General Plan Completed
12. Implementation of Road Maintenance Plan

SECTION XVII: BENEFITS

1. WORKERS' COMPENSATION.

- A. **Coverage.** All employees are covered by workers' compensation which provides medical reimbursement and disability benefits for job-related illness or injury. An employee does not accrue benefits while receiving workers' compensation payments. For exact compensation coverage, check the workers' compensation contract on file with the Mayor, or designee.
- B. **Use of Leave.** Employees may use accrued vacation or sick leave to make up the difference between workers' compensation benefits and their base pay.
- C. **Medical Attention.** If a life-threatening injury occurs, 911 should be called to

An employee must tell the doctor how, when and where the accident occurred. The doctor will complete a medical reports and copies of this report should be sent within seven (7) days to the insurance carrier, the Industrial Commission, and to the injured worker (Please Note: Do not submit doctor or hospital bills for on-the-job injuries or illness to the regular medical plan).
- D. **Initial Reporting of Illness or Injury.** Reporting the accident or illness is critical to qualification for payment under workers' compensation. If an employee is injured while on the job, no matter how minor, the circumstances should be reported immediately to the immediate available supervisor that same day. After Workers' Compensation Form 122 is filled out, a copy must be sent to the insurance carrier and a copy must be sent to the Industrial Commission within seven (7) days of the date of injury.
- E. **Reporting while off the Job.** While on leave because of a bona fide, on-the-job injury or illness, an employee must contact their supervisor or the Mayor on a weekly basis to report on their condition. Failure to provide the required medical status reports may result in revocation of the leave and/or immediate termination.
- F. **Return to Service.** All employees must return to work after the approval of the attending physician. A statement from the attending physician stating the employee is able to resume normal duties will be required before returning to work. Failure to return to work when directed may result in immediate termination. An employee who is able to return to work in light duty status may be required to work in a different department and perform duties not contained

[Enter Entity Name Here] Return To Work Program

- I. Policy: [Enter Entity Name Here] is committed provide a safe work environment to our employees. But if an employee becomes injured on the job, we will do everything we can to help the employee heal and return to work as quickly as possible. When employees are able to work and be a contributing team member, the injured employee heals faster, we are more productive and the morale of our entire organization is lifted.
- II. Workers Compensation Coordinator: [Enter Coordinator's Name Here] is our Workers Compensation Coordinator (WCC). [Enter Coordinator's Name Here]'s direct phone number is (XXX) XXX-XXXX, cell phone number is (XXX) XXX-XXXX, [Email]. The Workers Compensation Coordinator will help injured employees and their supervisors achieve the goal of helping injured employees get healthy and back to being a contributing team member.
- III. Medical Providers: If a life-threatening injury occurs, 911 should be called to access normal emergency care. Employees with routine, non-life-threatening injuries should be taken by their supervisor to:
 - a. [Network Provider Occupational Medicine Clinic address. List of Network Providers is attached.]
 - b. If the Network Provider is not available (after hours, etc.), call the Workers Compensation Coordinator to arrange medical care.
 - c. Employees must seek care from the provider designated by the WCC. Failure to do so may affect their workers compensation claim.
- IV. Injury Reporting: All injuries, no matter how minor, must be reported immediately to the employee's supervisor. Supervisors report these injuries to the Workers Compensation Coordinator, who begins a workers compensation claim and helps to arrange medical care. All injuries must be reported the day they occur. Failure to report injuries could jeopardize coverage of the injury.
- V. Post Injury Procedures: After receiving medical treatment, these steps must be taken:
 - a. Employee and his/her supervisor deliver all paper work from the medical provider to the Workers Compensation Coordinator.
 - b. WCC and the injured employee's supervisor review any restrictions given by medical provider with the injured employee's job description and determine if the employee's normal job meets the restrictions. If not, a Restricted/Light/Transitional Duty job will be assigned to accommodate the restrictions. (Sample light duty jobs are attached.)
 - c. Injured employees must comply with the restrictions they are given. Failure to do so could slow their recovery or cause further injury.
- VI. Restricted/Light/Transitional Duty: [Enter Entity Name Here] will accommodate restricted duty jobs for workers injured on the job. The WCC will work with the supervisor to design a work strategy that meets the injured employee's restrictions and accomplishes [Entity's Name]'s goals.
- VII. Follow Up: Injured employee's supervisor and the Workers Compensation Coordinator will regularly follow up with the employee and medical providers to make sure the employee is getting the care required, attending their medical appointments, complying with their restrictions and that any restricted duty assignments are helping the employee move closer to their regular job duties.
- VIII. Interaction With Adjusters: One of the best ways to help an employee get healthy and return to work quickly is to communicate with adjusters who manage the workers compensation injury claim. They have access to resources and have a vast knowledge in how to help injured employees get better. Utah Local Governments Trust has partnered with Constitution State Services (CSS) to adjust claims. They can be reached at 800.243.2490.

